

MALAYSIA CHAMPIONSHIP SERIES 2022

SUPPLEMENTARY REGULATION SPORTING REGULATION TECHNICAL REGULATION

CONTENTS

- A. SUPPLEMENTARY REGULATIONS
- B. SPORTING REGULATIONS

Art

- 1. **Definition of Event**
- 2. Title of Event and Date
- 3. Description of Event
- 4. Status & Permit Number
- 5. Eligibility of Competitors
- 6. Eligible Cars
- 7. Venue & Race Distance
- 8. Event Program
- 9. Entries
- 10. Points Score
- 11. Awards
- 12. Scrutineering
- 13. Success Weight
- 14. Drivers' Briefing
- 15. Race Formats
- 16. Starts Procedure
- 17. Practices & Qualifying
- 18. Grid Positions
- 19. Race
- 20. Stopping of Race or Practice
- 21. Re-Starting A Race
- 22. Changing of Engine
- 23. Finish of the Race & Parc Ferme
- 24. Safety Car
- 25. Full Course Yellow
- 26. Pitwall/Pitlane/Paddock Area and Parking Areas
- 27. Penalties
- 28. Protests
- 29. Interpretation of Regulations
- 30. General Code of Driving Conduct
- 31. Tyre
- 32. Advertising
- 33. Organiser's Rights
- 34. **Promoter's Rights**

C. TECHNICAL REGULATIONS

APPENDICES

- Appendix I Race Offices (1st Floor) - Precise Location of Offices
- Appendix II Scrutineering Bay & Parc Ferme
- Appendix III Art. 11.7 (Sporting): Success Ballast Mounting
- Appendix IV Art. 24 (Sporting): Advertising
- Appendix V Art. 25 (Sporting): Pitwall/ Pit lane/ Pits/ Paddock Areas & Parking Areas
- Appendix VI Entry Form
- Appendix VII Media Information

MALAYSIA CHAMPIONSHIP SERIES 2022 SUPPLEMENTARY REGULATIONS

A. GENERAL

Previously grouped together under the Malaysian Super Series championship banner, the new Malaysia Championship Series will showcase the finest Racing Cars in their battle of supremacy.

The Organiser and Series Promoters, organise the Malaysia Championship Series 2022 which will be open to cars from various different categories: T-Production (Touring) and M-Production (Modified), S-Production (Stock), Classic Car and other Invitational Series.

No.1 DEFINITION OF THE EVENT

Massed start road races for cars of Zone / International status as approved by MAM under the adjudication of:

The International Sporting Code (ISC) of the Federation Internationale de l'Automobile (FIA),
The National Competition Rules (NCRs) of the MAM,
The Sporting & Technical Regulations
Any Bulletins, Additional Supplementary Regulations (ASRs) and / or final instructions issued by the Organiser(s).

No. 2 EVENT TITLE AND PERMIT NUMBER

2.1 Malaysia Championship Series - Zone

Permit No.: TBA

No. 3 NAME AND ADDRESS

ORGANISER

Sepang International Circuit, Jalan Pekeliling, 64000 KLIA, Selangor, Malaysia
Tel : +603 8778 2200 Fax : +603 8783 1000

PROMOTER

Please refer to individual Race/ Series Sporting Regulations

No.4 INFORMATIONS ABOUT THE CIRCUIT

CIRCUIT

Circuit : Sepang Circuit
Length : 5.543 km
Direction : Clockwise
Pit Exit : After the Start/Finish Line on the right side of the track

PRECISE LOCATIONS AT THE CIRCUIT

Stewards' office	:	1st Floor of the Pit Building (Appendix I)
Race Control	:	1st Floor of the Pit Building (Appendix I)
Scrutineering	:	Pit No.3 except if requested otherwise by Promoter
Parc Fermé	:	Perdana Tarmac except requested otherwise by Promoter (Appendix II)
Official notice board	:	Access lane next to Pit 4 (Appendix II)
Media centre	:	1st Floor Pit Building

No. 5 EVENT PROGRAMME

Subject to confirm via Additional Supplementary Regulations

Friday

- ✓ Sporting and Technical Scrutineering
- ✓ Drivers' Briefing.
- ✓ Official Free Practice session.
- ✓ Official Qualifying Session

Saturday

- ✓ Race(s)

Sunday

- ✓ Race(s)

The Organiser reserves the right to hold the race late in the afternoon and early in the night, if necessary.

No.6 OFFICIALS

Provisional Race Officials: -

Stewards of the Meeting	:	TBA - Chief Steward TBA - 2nd Steward
Club Steward	:	To be Advised
Clerk of the Course	:	Mr. Fazli Mukhtar Affandi
Deputy Clerk of the Course I	:	Mr. Zulkiflie Zainuddin
Deputy Clerk of the Course II	:	Mr. Arul Nadesan
Secretary of the Meet	:	Ms. Syazana Abu Nawar
Chief Timekeeper	:	Mr. Haris Shamsee
Chief Medical Officer	:	Dr. Azlin Zainal Abidin
Technical Director	:	Mr. Hamzah Adlan
Chief Scrutineer	:	Mr. Erza Anas
Chief Marshal	:	Mr. Azmi Ariffin
Permanent Starter	:	Mr. Kalaivanan

No. 7 DRIVERS' BRIEFING

It is compulsory for all drivers to attend the Drivers' briefing for the event. The Organiser will confirm the exact time and venue of the briefing thru ASR.

Series : TBA
Venue : TBA
Date : TBA
Time : TBA

No. 8 INTERPRETATION OF REGULATIONS

Kindly refer to respective Race/ Series Sporting Regulations.

No. 9 GENERAL CODE OF DRIVING CONDUCT

Kindly refer to respective Race/ Series Sporting Regulations.

No. 10 MONETARY FINES

For any monetary penalty (fine) decided by the Stewards of the Meeting, the payment must be made in Ringgit Malaysia regardless of the decision made in other currency.

No. 11 RACING FLAG

Yellow Flag

The yellow flag is shown in case of any danger, which could be due to a collision, or due to a mechanical or an electrical failure.

The way a yellow flag is waved also plays a crucial role and has different meanings. The driver must slow down at the sight of a yellow flag, and double waved yellow flags indicate that the driver must be prepared to stop until the situation is resolved.

It is accompanied with a SC board sign if the Safety Car is on track.

Overtaking is prohibited in all the cases.

Red Flag

The red flag is shown to halt a session. Bad weather conditions, poor track conditions or an accident on the track warrants the use of a red flag.

Blue Flag

A blue flag is shown to a car that is about to be lapped by a faster car. The slower car must allow the faster car to pass before the show of three blue flags or risk being awarded a penalty.

Moreover, the blue flag is also shown at the exit of the pit lane to warn the driver of a faster car present on the racing line.

Green Flag

Green flags are waved to indicate drivers to resume racing. It is shown when the driver has passed a potential danger or after the track has been cleared of debris. Drivers are free to overtake and resume race pace.

Black Flag

The black flag is accompanied by the car number and indicates that the driver is disqualified from the race. The driver is directed to return to the pit and will take no further part in the race.

White Flag

White flag indicates a slow-moving vehicle on track including ambulance (if any).

Chequered Flag

The chequered flag is waved at the end of a session. The race leader is shown the chequered flag at the end the race.

The chequered flag is also waved at the end of the allotted time during practice and qualifying sessions which indicates the end of the session.

Do not cross more than 1 time.

Black with Orange Disc Flag

This flag indicates that the driver has a mechanical issue and must return to the pits. Like the black flag, this one too is accompanied with a car number.

Half Black / Half White Flag

The black with orange flag is shown to warn the driver of unsportsmanlike conduct. Not mending their ways on the track could ultimately lead to a black flag.

Yellow and Red Striped Flag

The Yellow and Red flag indicates a deterioration of the track grip due to water, oil and other lubricants.



MALAYSIA CHAMPIONSHIP SERIES 2022

B) SPORTING REGULATIONS

This document (hereinafter collectively referred to as the "Regulations") has been issued and updated on 18th April 2022. Successive editions can be issued for supplementing and/or amending. The new editions will be called Additional Supplementary Regulations (ASR) or Bulletin, dated and issued to all relevant parties.

ASR/ Bulletin needs to be read together with the Supplementary, Sporting & Technical Regulations.

Notification

- (i) *Acceptance of an entry does not guarantee a start in the race. Drivers must qualify for a grid position in accordance with these Regulations.*
- (ii) *It is the Competitor's responsibility to ensure full compliance with these Regulations.*
- (iii) *Each Competitor and Driver agrees generally to promote goodwill towards the Series, the Organiser and all Competitors, persons and companies involved in the Series.*
- (iv) *In interpreting any Regulation the word 'his' is deemed to include 'her' and persons referred to in the singular includes, where the context so admits, the plural.*

Art. 1 ORGANISER / PROMOTER

Chief Executive Officer
 Sepang International Circuit Sdn Bhd
 Jalan Pekeliling, 64000 KLIA, Sepang
 Selangor, Malaysia
 Tel: ++603-8778 2221
 Fax: ++603-8783 1020

Art. 2 TITLE OF EVENT AND DATE

2.1 MALAYSIA CHAMPIONSHIP SERIES 2022

ROUND	DATES	FORMAT	REMARK
Round 1	20 th – 22 nd May	1-hour Endurance	
Round 2	17 th – 19 th Jun	1-hour Endurance	
Round 3	22 nd – 24 th July	1-hour Endurance	
Round 4	16 th – 18 th September	300km Endurance	

Each category will be run for only maximum of Four (4) rounds for the season.

- 2.2 The Organiser reserves the right to abandon, cancel, postpone or amalgamate the dates of the event without prior notification in the case of not receiving more than 6 entries in each category or due to unforeseen circumstances.

Art. 3 DESCRIPTION OF EVENT

A maximum of four (4) rounds Championship for cars of various categories/ classes, specified hereunder:

- 3.1 T - Production (TP)**
- 3.2 M - Production (MTC)**
- 3.3 S – Production 1 & 2 (SMax 20 & SMax 16)**
- 3.4 Classic Car**

For the technical specifications of each Category, please refer to the notes in Section II of the Technical Regulations.

Art. 4 STATUS & PERMIT NUMBER

- 4.1 Zone. Permit number to be advised later.
- 4.2 This competition is held under the International Sporting Code of the FIA incorporating National Competition Rules of the Automobile Association of Malaysia, and these Supplementary Regulations and Additional Supplementary Regulation issued from time to time.

Art. 5 ELIGIBILITY OF COMPETITORS

5.1 Each entry can have up to a maximum of 2 drivers. The eligibilities of competitors are as follows:-

- a) Competitors in possession of a valid competition licence issued by the Motorsports Association of Malaysia (MAM) for Circuit Racing. (Minimum competition licence required: National “C” (Novice))
- b) Competitors in possession of a valid competition licence issued by the respective FIA - National Sporting Authority (ASN) accompanied by a letter or visa of approval from the same to participate in the event.
- c) Any Malaysian team having foreign driver(s) must hold the Motorsports Association of Malaysia (MAM) entrant/team licence.

d) Competitor from outside the zone not eligible to score point(s)

- 5.2 Each competitor can only drive 1 car per category.
- 5.3 Each entry may be driven by a single or 2 drivers. During practice & qualifying, up to 2 drivers may drive one and the same car. During the race, a minimum of one (of the two entered) drivers may drive one and the same car.
- 5.4 When an entry includes two drivers, the Team/ Competitor to designate “Driver 1” and “Driver 2” at the end of the administrative checks. The designation (Driver 1 and/ or Driver 2) determines in which race the driver will compete.

- 5.5 Should a driver depart either to another team or to another class, all driver classification points awarded to the driver up and till the time of departure will be deleted and the driver will start again from zero.
- 5.6 The organiser reserves the right to reject any entry in accordance with the National Competition Rules (NCR) of the MAM. There is no appeal against their decision.

Art. 6 ELIGIBLE CARS

Kindly refer to the respective Technical Regulations.

Art. 7 VENUE& RACE DISTANCE

Venue : Sepang International Circuit
Length : As specified in the Additional Supplementary Regulations of each round.
Race Duration/ : 1-hour Endurance race with a 3 minutes compulsory pit stop; or
Distance : 300km Endurance with 2 x 5 minutes compulsory pit stop and refuelling;
or
10 laps Sprint race

Subject to confirmation via the Additional Supplementary Regulations.

Art. 8 PROVISIONAL EVENT PROGRAMME

The event program will be declared in the Additional Supplementary Regulations of each round. The Series / Championship will normally run according to the following type of schedule which, however, may still be modified to suit the needs of the Organisers and/or as indicated in the Provisional Event Programme

Wednesday and Thursday

When possible, Paid Practice sessions will be organized. They will be later communicated to Drivers by the Organiser via email

Friday

- ✓ Sporting and Technical Scrutineering
- ✓ Drivers' Briefing.
- ✓ Official Free Practice session.
- ✓ Official Qualifying Session

Saturday

- ✓ Race 1 (1 Hour Endurance)

Sunday

- ✓ Race 2 (1 Hour Endurance)

The Organiser reserves the right to hold the race late in the afternoon and early in the night, if necessary.

Art. 9 ENTRIES

- 9.1 Entries are open upon posting of these Regulations and close on the Wednesday preceding each scheduled round or as otherwise stated in the ASR. Entries received on Thursday onwards will be considered as late entries. Acceptance of entries is at the sole discretion of the organisers who have the right to reject entries without giving reasons. **(Appendix VI)**
- 9.2 Competitors are responsible for sending correct and complete entries with the correct entry fees prior to the entry closing dates.
- 9.3 Competitors providing incorrect or incomplete entries, even though they have paid the entry fees (including driver to be nominated entries), are to be held in abeyance until they are complete and correct and the date of receipt for acceptance of entry purposes shall be the date on which the Secretary of the Meeting receives the missing or corrected information.
- 9.4 It is a condition of entry that competitors have no right to any claims against the Motorsports Association of Malaysia, Promoter, Organiser, Sponsors and/or any one carrying out their duties.
- 9.5 Please submit entries to:

Secretary of the Meet
SEPANG INTERNATIONAL CIRCUIT SDN BHD
Jalan Pekeliling, 64000 KLIA, Selangor
Tel: +603-8778 2221
Fax: +603-8778 2338
E-mail: syazana.abunawar@sepangcircuit.com

Late entries may be accepted at the discretion of the organiser and approval by the Steward of the Meet at double entries fee.

- 9.6 Maximum number of entries accepted will be at the sole discretion of the organisers. The Organiser reserves the right to refuse an entry. If less than six (6) entries are received for any category, the Organiser reserves the right to cancel or amalgamate the said category with any other categories.
- 9.7 The number of cars allowed to start the race is based on the track homologation. Priority will be given to competitors who enter and pay the entry fees for the full Championship Series in advance of the first event. However, the organizers reserved the rights to change the composition as it deems fit.
- 9.8 Entry fee per round is as follows:

	<u>Full Season (4 rounds)</u>	<u>Per Round</u>
T- Production (TP) & S-Production 1 (SMax 20)	RM5,400	RM1,500
M-Production (MTC) & S-Production 2 (SMax 16)	RM3,960	RM1,100

Any entry for which the entry fees have not been received until the entry closing date or for which the entry fees have not been paid completely can be regarded null and void.

- i) Payment must be made by cash, credit card, money order or banker's draft. Cheque may be accepted at the discretion of the organisers and made payable to **Sepang International Circuit Sdn Bhd**

However, payment made on-site on official registration day must be in cash or credit card.

- ii) Payment can also be made through the following bank account but the receipt must be presented to the Secretariat during registration as proof of payment: -

Account Name: Sepang International Circuit Sdn Bhd
Account No.: 5124 3730 0322
Bank Name: Malayan Banking Berhad
Bank Address: Southern Support Amenities
Jalan Pekeliling, 64000 KLIA
Selangor, Malaysia
Bank Code: MBBEMYKL

The entry fee will not be refunded under the following conditions: -

- i) Entry(s)/ competitor(s) who is disqualified from the race due to any type infringement.
ii) After Official Registration closed.
iii) Full season payment.

The entry fee may be refunded due to Refusal of Entry by Organiser or Stewards.

Art. 10 POINTS SCORE

- 10.1 Points scores for each category will be awarded to the entries as follows:

Position	Points
1 st	15
2 nd	12
3 rd	10
4 th	8
5 th	6
6 th	5
7 th	4
8 th	3
9 th	2
10 th	1

- 10.2 No points will be awarded in case 20.1A if the race cannot be re-started.

- 10.3 Half the number of points will be awarded in case 20.2B if the race cannot be re-started.

- 10.4 Full points will be awarded in case 20.3C.
- 10.5 Points for Championship will be determined based on the combination of the results of each round. In the case of a dead heat at the end of the Championship, the winner will be the entry with the highest number of race wins. If still tied then account will be taken of the highest number of second places and the highest number of third places, etc.
- 10.6 Points will be awarded to the entry and will stay throughout the championship as long as the entry competes in the same category.
However for any entry with 2 drivers, points will only be given to Driver 1 as registered in the entry form should the entry split in the mid-season.
- 10.7 In 300km Race (MCS Round 4), points will be multiplied by 2.

Art. 11 AWARDS

- 11.1 At the end of season, Overall championship will only be awarded if the category / class compete in every round. Championship awards for cash and trophies:

Category	1 st	2 nd	3 rd
T - Production	Trophy & RM11,000	Trophy & RM9,000	Trophy & RM8,000
M - Production	Trophy & RM10,000	Trophy & RM8,000	Trophy & RM6,000
S- Production 1 & 2 (SMax 20 & SMax 16)	Trophy	Trophy	Trophy

Prizes:

- 11.2 Trophies will be given for the top 3 in each round. No cash will be awarded for each round.
- 11.3 ENGINES may be STRIPPED for legality after qualifying or races at the discretion of the organisers. Competitors to provide mechanics and specifications of car.
- 11.4 The podium ceremony **may** be held after the completion of the event for each round. It is compulsory for the winners to be available for the podium ceremony for each round with full gear except for helmet, gloves and HANS device.
- 11.5 At the end of the Championship season, an Award Ceremony may be held to honour all the winners and it is compulsory for all the winners to attend, failing which their prize money may not be released in full.

Art. 12 SCRUTINEERING

- 12.1 All cars must comply with Section C – Technical Regulations. Refer to the said regulations for each category. The homologation papers must be presented during the Scrutineering together with drivers racing suit, helmet, gloves, shoes and HANS device.
- 12.2 All stickers required as stated by the Organiser must be properly placed on the car prior to the scrutineering. **(Refer to Appendix IV).**
- 12.3 All cars and helmets will be issued with an “OK” sticker by the Scrutineers once the scrutineering has been successfully completed.
- 12.4 All cars will be impounded immediately after the qualifying session for weighing. The official weighing scales readings are final. No protest will be entertained on this matter.
- 12.5 Throughout the entire duration of the event, the cars must comply with the Technical Regulations in all points. Engines may be sealed and the organiser may impound cars.
- 12.6 Any car, which, after having passed scrutineering, is damaged, must be re-presented to the Scrutineers after repair and be approved in order to be allowed to continue.
- 12.7 The Clerk of the Course reserves the right to impose additional weight penalties to be carried on a car in the interest of equalisation of performance.
- 12.8 All participants **MUST** carry their own transponder for timing. The transponder unit must be compatible to the system used by the Organiser. If for any reason, the participant does not possess their own transponder, a rental rate of RM200 plus RM200 for deposit will be charged for using the Organiser’s transponder.

Art. 13 SUCCESS WEIGHT

- 13.1 A system of “Championship success ballast” weight handicap will be applied to the top 3 drivers in MCS by Clerk of the Course at the end of the Race 2 at each Meeting based on the order in the Drivers Championship published by the Organiser as authorised by the Clerk of the Course prior to any judicial action in respect of any Sporting or Technical issue. The Championship success ballast will not be applicable for Round 1 of the Championship.

The Championship success ballast weight awarded after Round 1 Meeting will be effective for the Free Practice session, Qualifying session and the Races at the following Championship Meeting.

Should two or more drivers be tied on points, those drivers shall be awarded equal Championship success ballast equivalent to that of the lowest position that has been removed due to the tie.

The following weight ballast will apply:

1 st Position	-	30kg
2 nd Position	-	20kg
3 rd Position	-	10kg

Only the mandatory Championship success weight ballast will be provided by the **Organiser** and must be fixed inside the car in a safe manner utilising the mounting **procedure**. In the event of ballast needing to be removed or exchanged, **ONLY** the official Scrutineers may supervise or carry out that procedure.

Ballast must be secured to the shell/chassis via at least 4 mounting points using bolts with a minimum diameter of 8 mm each with steel counter plates of at least 400 sq.mm surface area and 3 mm thickness (**Appendix III**).

Between events, ballast is allocated according to championship positions and is carried out in qualifying and races.

The weight must be sealed and positioned in accordance with the provisions of that same Article. It must always be clearly identified by marking or by painting
There will be no accumulation of success ballast weight.

In keeping with the principle within the championship/ series, if a new Competitor(s)/ Entry(s) joins the Championship/Series in Round 3 Event onwards, he will be required to carry additional weight of up to 30kg at the first Race/Round in which they participate from the official minimum weight (eg. TP= min. 1130kg, MTC= min. 1030kg...so forth).

- 13.2 Additional Pit Stop Time will be applied to drivers on the order of finishing published from every official race result. Must be served during compulsory pit stop of the race.

Winner	-	15 sec.
1 st Runner-up	-	12 sec.
2 nd Runner-up	-	08 sec.

Art. 14 DRIVERS' BRIEFING

- 14.1 It is compulsory for all drivers to attend the Drivers' briefing for the event. The Organiser will confirm the exact time and venue of the briefing thru ASR or bulletin.
- 14.2 The Clerk of the Course reserves the right to organise a special drivers' briefing in addition to the compulsory briefing.

Art 15 RACE FORMATS

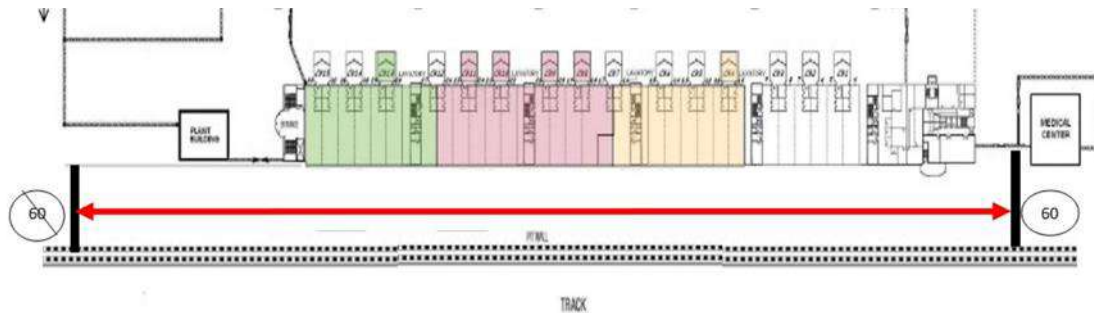
Each round will be run with either one of the following formats. : -

15.1 Format 1

- 2 races of a 60 minutes duration plus the active leader with a **COMPULSORY** 3 minutes pit stop. Pit Stop is mandatory for the all cars during the race and to be carried out between the 25th and 40th minutes from the official start time. Minimum pit stop time is calculated between pit entry loop and pit exit loop. For teams with 2 drivers, the change of driver is during this pit stop.
- The pit stop must be carried out in front of the designated pit or area of each team.
- No re-fuelling is allowed.

15.2 Format 2

- 300km race with a compulsory 2 times 5 minutes pit stop, which must be carried out during the period between 12th laps and 45th laps of the race, from the start of formation lap. Change of driver is to be carried out during this pit stop.
- The pit stop must be carried out in front of the designated pit or area of each team.
- Re-fuelling is only allowed with organiser fuel rig.



Note: Minimum pit stop time is calculated between pit entry loop and pit exit loop (pit lane boundary).

15.3 Format 3

- Will have to two (2) sprint races of a maximum of 10 laps each or 30 minutes.
- There will no compulsory pit stop for each race.

Art. 16 **START PROCEDURE**

General:

The Pit Lane will open by signal of a waved green flag and an audible signal, and will remain open for a period of 5 minutes, whereupon drivers must exit the Pit Lane and go straight to their specific grid box. The only boards will be shown are 5 minutes (when Pit Exit opens) and 2 minutes (before the Pit Exit closed).

After the Pit Exit closed, a ten (10), five (5), three (3), and one (1) minute and thirty (30) seconds warning signal accompanies with a signal board will be shown to the Grid.

Any cars removed from the grid after the Pit Lane is closed and before the Green Flag of Formation Lap shall be held in the Pit Lane to be started at the rear of the entire grid after the last car has passed the Pit Lane Exit at the start of the Formation Lap.

When the 3-minute board signal is shown, all cars must have their wheels fitted and the car must be resting on their wheels, after this a single wheel may only be removed in the pit lane, or on the grid during a race suspension.

After the three (3) minute signals, everyone must vacate the grid except for Driver in car, Officials and 2 team mechanics.

When the 1-minute board signal is shown, engines should be started and all team personnel must leave the grid taking all the equipment with them. If any team personnel or team equipment remain on the grid after the 30 second signal has been shown, the driver of the car concerned may be penalised for Starts Procedure Infringement by the Steward.

Any drivers unable to start the Formation Lap are required to signal the marshals (put up his arm to warn the starter and the following competitors of the problem) and will be pushed into Pit Lane to start from Pit Exit.

Overtaking during the formation lap is only permitted if a car (self-started) is delayed when leaving its grid position and cars behind cannot avoid passing it without unduly delaying the remainder of the field. In this instance, drivers may only overtake to re-establish the original starting order, but must return to their original starting position before reaching the Safety Car Line 1. If the driver fails to return to their original starting position before reaching the Safety Car Line 1, then the driver must enter the pit lane and start from the pit exit. If this situation involves multiple cars, then the cars will line up in the same position at the time in which they enter the pit lane. However, should any driver who is delayed leaving his position may not overtake another moving car if he was stationary after the Medical Car had crossed the Start Line, and must start the race from the back of the grid. Any driver refusing to comply will result in a penalty as deemed appropriate by the Stewards.

16.1 Rolling Start

- Cars will form up in pairs on the back straight between turns 14 and 15.
- The official car will enter the pits if the Clerk of the Course is satisfied with the formation, otherwise it will carry on for extra lap(s).
- Drivers in their cars must keep the formation as tight as possible maintaining the distance from the cars in front until the start signal has been given. At the end of the (last) formation lap, the Pace Car will pull off before the start line. The car in Pole Position must maintain its speed until the start signal is given. Unless otherwise specified in the briefing, the start signal will be given by turning off the red lights.
- All the cars will maintain the pace up to the start line and will commence racing when the red light turns to green. Overtaking is allowed when the red light goes-off.
- Timing of the race will commence once the Starter wave the Green flag at the start of Rolling Lap.

16.2 Standing Start

- As soon as all cars are in position on grid at the end of the Formation Lap, the 5 second board will be displayed. 5 seconds after that board is withdrawn the red lights will be switched on and between 2 and 7 seconds later the lights will be switched off to signal the start of the Race.

FLAG START

Should there be a situation failure of the Startlight, Flag Start board will be shown before the 1 minute board at Starters Podium. The race deems to start if the Starter raise the Green Flag from up to down from the Starters podium.

The start procedure and/or countdown procedure may be varied to accommodate commercial demands and competitors will be informed of the changes at the Drivers Briefing and/or through the issuing of a Bulletin and/or via the timing screens where possible.

Art. 17 FREE PRACTICE & QUALIFYING SESSIONS

- 17.1 There will be a maximum of 1 x 30 minutes official free practice session at each race meeting plus maximum of 1 x 20 minutes official qualifying session held at each race event.
- 17.2 During all sessions there will be a green and a red light at the pit exit. Cars may only leave the pit lane when the green light is on. Additionally, a blue flag and/or a flashing blue light will be shown at the pit exit to warn drivers leaving the pits if cars are approaching on the track.
- 17.3 Clerk of the Course with the consent of the Steward may interrupt Free Practice as often and for as long as he thinks necessary to clear the track or to allow the recovery of a car. The Clerk of the Course with the agreement of the Steward may decline to prolong the Free Practice period after an interruption of this kind.
- 17.3 In Qualifying Session the Clerk of the Course may interrupt the session as often and for as long as he deem necessary to clear the Track or to allow the recovery of a car. Whenever possible, the time lost by the suspension of a Qualifying Session shall be recovered so that the minimum scheduled period for qualifying may be accomplished. However, it is not obligatory to resume the session or re-run the session to achieve the time criteria. The decision of the Stewards in this matter shall be final. No protest can be accepted as to the possible effects of the interrupted/shortened session on the qualification of any Driver.
- 17.4 It is compulsory for every driver to take part in qualifying session. Failure to take part in Qualifying session, the competitor(s) must write an application to start the race to Clerk of the Course.
- 17.5 Only the Stewards of the Meeting may approve any competitor(s) who did not qualify/ participate in qualifying session to start from the back of the grid on condition that:
- They do not eliminate vehicles that have already qualified.
 - They are judged capable of achieving the qualification minimum.
 - The competitors satisfy all safety requirements, including knowledge of the circuit.

Art. 18 GRID POSITIONS

- 18.1 At the end of the qualifying practice session, the list of the qualified drivers will be published.
- 18.2 The grid positioning of the cars will be formed by the categories/ classes.
- 18.3 The starting grid for Race 1 will be drawn up in the order of the fastest time achieved by each car in the qualifying session. Should two or more cars have set identical time, priority will be given to the one, which set it first.
- 18.4 Race 2 starting grid will be based on the result of Race 1.
- 18.5 Any competitor whose car is unable to start for any reason whatsoever (or who has good reason to believe that the car will not be ready to start) must inform the Clerk of the

Course accordingly at the earliest opportunity in any event, no later than 30 minutes before the start of the race. If one or more cars are withdrawn, the grid will be closed up accordingly.

- 18.6 Access to the grid will close 5 minutes before the scheduled formation lap. Any competitors that have not taken up his/her grid position by then will start the race from the pits after the last vehicle has passed the pit exit on the first lap of the race.

Art. 19 RACE

- 19.1 The time duration of the Race must not be changed unless authorized by the Stewards.
- 19.2 During each Race, a Driver must not drive any more than forty (40) minutes during a race of sixty (60) minutes in time. At each race, a driver is required to enter the Pit Lane to undertake a driver change and/or be stationary for a minimum of 3 minutes when pit window opens (Whether or not driver change occurs), which is a metric to promote parity among the teams except driver(s) incur Compulsory Additional Pit Stop Time.
- 19.3 During the Race the pit window will occur from 25-40 minute duration. Driver swaps and stationary time can take place only during this time. Driver swaps or stationary time must not occur at any other time.
- a) Failure to enter the pit-lane and be stationary during the pit window (an entry must cross the pit entry control line before pit window closes).
 - b) Failure to complete a driver swap during stationary time.
 - c) Failure to stop less than the stipulated stationary time.
 - d) Complete a driver swap before or after pit window occurs.

Any violation of Article 19.3 a, b, c, or d, will result in the Stewards imposing penalty.

- 19.4 If the pit window occurs under safety car conditions, the pit lane will remain open and drivers may complete their stationary time or driver change. Drivers leaving the pit lane after having completed stationary time or driver change may only do so when the pit exit light is green. If the safety car is still deployed at this time, drivers leaving the pit lane must remain at a safe speed and line up after the safety car or the back of the formation.
- 19.5 During the race, drivers leaving the pit lane may only do so when the pit exit light is green.
- 19.6 After a car has passed scrutineering and the Stewards have authorized it to start a race, it may do so once the Pit Lane has opened during the Race.
- 19.7 Fuelling/ Refuelling is prohibited on the grid and during the 1-hour race.
- 19.8 The car's headlights and rear lights must be illuminated at all times when the track has been declared wet. The Clerk of the Course may require cars that fail to comply with this article return to the pit lane to fix.

- 19.9 The lollipop man and two (2) crew may wait in the Inner Lane up to one lap before the car stops for a pit stop (excluding the driver and the co-driver). Other than this and when the car is stopped for its pit stop all personnel must remain inside the pit behind the pit lane line (pit garage line) or at the pit wall (where passes authorized them to do so).
- 19.10 At the time the car is in the pit lane the engine must NOT be running unless all four wheels are on the ground. The car's engine may be restarted only when the car has being lowered and all four wheels of the car are in contact with the ground and is ready to re-join the race.
- 19.11 For each race a reference time for the Mandatory Pit Stop will be established taking into account the time taken to drive at a maximum speed of 60 km/h in the pit lane from the pit entry timing loop to the pit exit timing loop, the completion of the mandatory stop for the driver change (taken as 3 minutes) and the driver compulsory additional pit stop time.
- 19.12 Penalties may be imposed at the discretion of the Stewards for an unsafe release during any session of the event.

Art. 20 STOPPING OF RACE OR PRACTICE

- 20.1 Should it become necessary to stop a race or practice by the Clerk of the Course or his Deputy, it will be done with the red flag at the start line and all flag posts.
- 20.2 During practice, all vehicles will proceed slowly to the pits. During a race all vehicles will proceed slowly to the grid for a re- start.

Art. 21 RE-STARTING A RACE.

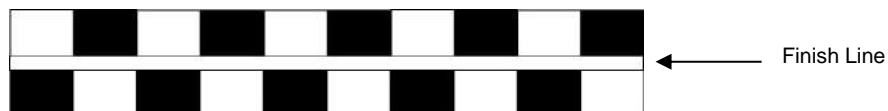
- 21.1 Case A: Less than 2 full laps.
- The original race will be deemed null and void.
 - The race will be run in full if it can be re-started.
 - All qualified competitors will be allowed to re-start the race. No spare car will be allowed. Original cars may be worked on.
- 21.2 Case B: More than 2 full laps, but less than **60%** of race distance/ duration.
- The race shall be deemed in two parts. The first of which will be the penultimate lap the leader crossed the finish line.
 - The length of the second part will be two laps less than the scheduled race distance.
 - Only vehicles that took part in the first start will be eligible if they return to the pits under their own power.
 - Vehicles may not be worked on or refuelled in the pit or grid.
 - The start will be from the 10 minutes procedure.
- 21.3 Case C: 60% or more of the race distance/duration.
- The race will be deemed to be finished.
 - The results will be based on the penultimate lap that the red flag was shown.

Art. 22 CHANGING OF ENGINE

Any entry that requires change of engine after qualifying will lose their grid position. They will have to submit their application to the Steward to start at the back of the grid. Change of engine is allowed between Qualifying, Race 1 and Race 2 and the spare/replacement engines must be eligible, scrutineered and sealed before qualifying session.

Art. 23 FINISH OF THE RACE & PARC FERME

- 23.1 Chequered Flag will indicates the end of race/session signal and will be given at the Starter's Podium as soon as the leader has covered the full number of laps or elapsed time.
- 23.2 Should for any reason the race signal be given before the leader completes the scheduled number of laps, the race will be deemed to have finished when the leader last crossed the finish line before the signal was given.
- 23.3 Any classified vehicle that cannot reach the 'parc ferme' under its own power will be placed under the exclusive control of the marshals who will take it to the 'parc ferme'.
- 23.4 All drivers who started will be classified if they have covered at least 60% of the distance (rounded down to the nearest whole number of laps) covered by the winner.
- 23.5 Once the race is finished, drivers must take the chequered flag on the track to be considered as finisher. The leader will be shown the chequered flag when he crosses the finish line at the end of the lap during which the number of laps or total time is exceeded (refer image below).



Art. 24 SAFETY CAR

- 24.1 In the event of incidences one (1) safety car will enter the track from the pit exit. The safety car will be brought into operation and run under Art.23 and in accordance with Appendix H, Article 2 (2.10) of the FIA International Sporting Code. The safety car will be deployed only in the race and not other sessions.
- 24.2 When the Safety Car is on the track, all flag posts will display yellow SC Boards together with yellow flags/ boards. Cars must proceed at an appropriate speed until it reaches the end of the line of cars behind the safety car.
- 24.3 All Cars must then form a line behind the Safety Car, no more than ten car lengths apart and overtaking, with the following exceptions, is forbidden until the Cars reach the Line after the Safety Car has returned to the Pits.

Overtaking may be permitted under the following circumstances:

- a. if any Car slows significantly with an obvious problem (blue flag will be given);

- b. under the provisions of FIA Article 23.8 (Safety Car Start).
- c. should any car reported slowing down during Safety Car period, whether it is deliberate, having mechanical issues or driving at a speed of less than 50km/h, will be then overtaken. That particular driver will be investigated by the Stewards for any penalties.

24.4 For the avoidance of doubt the overall leader will have to maintain position behind any Car in front on track. The Safety Car will not employ the “green light” wave by system so Cars between the leader and the Safety Car and also backmarkers will not be allowed to overtake the Safety Car.

Anyone found to have overtaken any competitor in front will receive a drive through penalty, only after the safety car has been withdrawn from the track. If the driver does not pit within 3 laps, the black flag will be shown for exclusion of the team from the race.

24.5 Drivers may pit during Safety Car procedure and re-enter the track under green pit exit light. All pit-in laps will be counted in the overall results.

24.6 When the track is cleared, the safety car will turn off the flashing lights at Intermediate 3 and return to the pit.

24.7 When the safety car returns to the pit, racing and overtaking will resume after crossing the Control Line (Start/Finish). The lead car to maintain the pace up to this point. Anyone overtaking before the line will incur a drive-through penalty.

24.8 Under certain circumstances the Race may be started behind the Safety Car. In this case, at any time before the “one-minute” signal is given, its orange lights will be switched on. This is the signal to the Drivers that the Race will be started behind the Safety Car. When the green lights are illuminated, the Safety Car will leave the Grid with all Cars following in Grid order, no more than ten car lengths apart. There will be no Formation Lap and the Race will start when the green lights come on.

Overtaking, during the first lap only, is permitted if a Car is delayed when leaving its Grid position and Cars behind cannot avoid overtaking it without unduly delaying the remainder of the field. In this case, Drivers may only overtake to restore the original starting order.

Any Driver who is delayed leaving the Grid must not overtake another moving Car if they were stationary after the remainder of the Cars had passed them. This Driver must then take up position at the back of the line of Cars behind the Safety Car. If more than one Driver is affected, they must take up position at the back of the field in the order in which they left the Grid.

24.9 No penalty(s) (Drive-thru, pit stop, etc.) can be served during Safety Car period.

Art. 25 FULL COURSE YELLOW

Should it be necessary for Safety reasons, whenever a car is stopped in a dangerous location, whenever track vehicles are working near the track, marshals are working close to the track or other similar situations, Clerk of Course will declare a Full Course Yellow procedure.

- 25.1 The instruction of "Full Course Yellow" will be presented through the Flag Marshal at every respective official Flag Post around the circuit. This procedure will ONLY be used during day light. For night interventions, Safety Car will be implemented.
- 25.2 The FCY boards will be presented at every flag post to inform cars that they must slow down to 80km/h. Overtaking is forbidden from the moment that FCY boards are displayed. All flag Marshal will wave yellow flags and FCY board. At this point, all cars must be at a constant speed of 80 km/h.
- 25.3 Speeds of every car will be monitored through transponders by the circuit Official timekeeping system. The speed detection results will be submitted to stewards for investigation and any car completing a full lap under FCY conditions, faster than this given time will incur a penalty. Official transponder detection is final and cannot be protested.
- 25.4 The pit lane entry and exit will remain open. Cars entering the pit lane may overtake cars that are on the track once they cross the Safety Car Line 1.
- 25.5 Once the problem is resolved, the track will return to Green and every Flag Marshals post around the track will wave Green Flags. Alternatively, should the problem not be resolved, the Safety Car may be deployed.
- 25.6 The Safety Car will overtake all cars on the track and locate the overall leader. Once this has been ascertained, the amber yellow lights on the Safety Car will be switched on, indicating the start of Safety Car procedure.
- 25.7 As soon as the Safety Car is in front of the overall leader, all Flag Marshals will exchange the boards from FCY to SC Board. This is the indication that all cars can now proceed at above 80 km/h and at a reasonable speed and safe pace until reaching the line of cars behind Safety Car.

Art. 26 PITWALL/PITLANE/PITS/PADDOCK AREAS AND PARKING AREAS

- 26.1 Please refer to the illustration of the pit wall, pitlane, pits, paddock areas and parking areas in Appendix V.
- 26.2 Pit allocation will be made by the organiser. Decisions are final and the pit allocation will be published, once the final entries have been confirmed. There is no right to be allocated a specific pit. **Each pit will be shared by several teams with a maximum of 4 cars per pit. The competitors must conform to it and respect it.**
- 26.3 There is no claim on a special pit wall, paddock areas and parking areas. Access and allocation of areas will be made upon instruction of the officials and their instructions must be strictly respected.
- 26.4 It is the responsibility of each competitor/team manager to ensure that team members are fully conversant with all pit rules. Any contravention by any team member may entail the exclusion of the team from the race. To avoid allegations of misconduct, team

members are encouraged not to stray into other team's pits unless invited or with specific permission to do so.

- 26.5 Each competitor/ team manager is responsible for the people in his pit, even if they are not part of his team.
- 26.6 No car may be driven in reverse in the pit lane. **Heavy penalty including monetary fines or/ and immediate exclusion can be the result from such action.** Mechanics are allowed to push their cars backwards to their designated pit.
- 26.7 Smoking and drinking of beer or alcoholic drinks are strictly prohibited in all areas between the pit and the pit wall. A penalty of RM300 will be imposed on anyone caught breaking these rules. Persons caught will be ejected from these areas.
- 26.8 Each team will be entitled to five (5) update passes comprising of 1 competitor, 2 pit wall crew and 2 pit crew. These passes must be worn in a prominent manner at all times. Only those wearing pit wall passes will have access to the pit wall.
- 26.9 Compressed air and electric gun (checked & approved by scrutineer) can be used in pit/ pitlane.
- 26.10 In keeping with the status of the MCS as a premier event, teams are encouraged to be properly attired at all times. Minimum acceptable: -
Cotton T-shirt, Jeans, shoes and shorts. For safety reasons singlet, slippers and sandals are not allowed to be worn by pit crew.
- 26.11 Children under the age of 12 are not allowed in the pits, pit lane or pit wall until the race has ended. An exception is made for glassed hospitality area in pits. However, children are to be supervised by an adult at all times. Animals (of any kind) are not permitted in the circuit.
- 26.12 No spare car or any other form of vehicle may be parked in the pits. Any such vehicle found in the pit of any team will be reported to the Stewards of the Meet for penalty up to exclusion. Illegally parked vehicles will be towed away at the owner's costs and an additional penalty of RM300 will be imposed on the owner.
- 26.13 Pit to driver radio communication is permitted for all classes. **However, entrant / entry must apply/ submit the frequency 1 month before the event to the organiser for approval.**
- 26.14 **The use of motorized vehicles, skateboards or similar means of transportation by any parties in the pitlane area is strictly prohibited.** The organiser has the right to confiscate such vehicles until the end of the event and/or the Steward has the right to impose monetary fine.

- 26.15 Large umbrellas may be used along the signalling wall to protect from rain and sun. They must be securely tied to the railing along the pit signalling area.

Art. 27 PENALTIES

The following penalties can be applicable during the practice, qualifying and race. Other infringements not stated below, will fall under the jurisdiction of the Stewards of the Meeting. It is not permitted for the driver to enter the pit lane to take their drive through penalty during a Safety Car period. The Race Starter will be the Judge of Fact.

No	Infringement	Practice	Qualifying	Race
27.1	Unsafe release (during race)	RM100	RM200	Drive-thru or 60 sec. time added
27.2	Speeding in pit lane	RM100	RM200	Drive-thru or 30 sec. time added
27.3	Overtaking under yellow flag	RM100	RM200 + Remove time of that lap	Drive-thru or 30 sec. time added
27.4	Failure to be within grid box	n/a	n/a	30 sec. time added
27.5	Failure to enter the pit-lane and be stationary during the pit window	n/a	n/a	Exclusion (at the Steward approval)
27.6	Failure to stop less than the stipulated stationary time.	n/a	n/a	Stop & Go and complete the remaining time
27.7	Enter pit lane to perform the compulsory pit stop outside the pit window	n/a	n/a	Drive-thru or 60 sec. time added penalty
27.8	Bumping car in front (avoidable situation)	At Stewards discretion		60 sec. time added
27.9	Causing other car to retire	At Stewards discretion		Exclusion (at the Steward approval)
27.10	Not carrying out drive-through in 3 Laps	n/a	n/a	Exclusion (at the Steward approval)
27.11	Not observing black flag	At Stewards discretion		Exclusion + RM100.00 per lap
27.12	Provoking a fight at Pit/ Paddock/ Track by any of Competitor/Team member /Representative	Exclusion from race + Ban of Team and team members from SIC for 12 months + RM5,000.00		
27.13	Retaliating in a fight	Exclusion + RM3,000.00		
27.14	Illegal parking in Pit or at Paddock area	RM300.00 + towed		
27.15	Other offences	At Stewards discretion		

Note:
Steward(s) may enhance any of the above penalties.

Art. 28 PROTESTS

- 28.1 Protests, if any, must be lodged in accordance with the stipulations of the PART X of NCR. Under strict respect of the protest time of 30 minutes, all protest must be lodged in writing attention to the **Clerk of the Course** and **handed over to the Secretary of the Meet** or if not possible to the Assistant Secretary of the Meet along with the protest fee of:

Sporting	-	RM1,000.00
Stripping	-	RM2,500.00

- 28.2 Protest time is 30 minutes from posting of provisional results. The Organiser reserves the right to strip any car after the race at the cost of the Team/ entry.
- 28.3 A collective protest(s) and/or any protests against decisions of the timekeepers and/or the judges of fact, are not permitted.
- 28.4 Each Competitor/Driver must remain available and easily contactable at an Event until any Protest and/or Appeal period relating to the Championship has elapsed, failing which, any judicial action against or relating to that Competitor and/or Driver may be heard in their absence.
- 28.5 Incomplete protest will not be entertained.
- 28.6 A successful protest shall merit a refund of the protest fee.
- 28.7 Appeals must be according to PART XI of NCR.

Art. 29 INTERPRETATION OF REGULATIONS

- 29.1 Only the Clerk of the Course can give binding information about the event, or in his absence, his deputy.
- 29.2 In the case of any dispute, the interpretation of this Sporting & Technical regulation, the Additional Supplementary Regulations is up to the Clerk of the Course.
- 29.3 The Organiser reserves the right to modify or supplement the present regulations if considered necessary for reasons of safety, force majeure or by order of the authorities or to cancel the event in case that extraordinary circumstances should arise, without any obligations for indemnification. Furthermore, the organiser holds liability only in that case where exclusion from liability is not mentioned in the regulations or entry form.

Art. 30 GENERAL CODE OF DRIVING CONDUCT

- 30.1 It is a requirement that a fair conduct by everyone involved and in particular by the drivers during the practice/ qualifying sessions and during the race. Drivers of faster cars are asked to show consideration and fairness towards the slower cars and vice versa. The driver must drive his car alone and unaided.

Overtaking, according to the circumstances, may be carried out either on the right or on the left. However, manoeuvres liable to hinder other Drivers such as more than one change of direction to defend a position, deliberate crowding of a Car beyond the edge of the Track or any other abnormal change of direction, are strictly prohibited. Any Driver who appears guilty of any of the above offences will be reported to the Stewards and may be penalized.

A driver who abandons a car must leave it in neutral or with the clutch disengaged and with the steering wheel in place.

- 30.2 Any driver obstructing or endangering other participants during practice or race due to their driving style or apparently not being up to the requirements of the race may be refused the start or excluded from the race.
- 30.3 Should a driver be obliged to stop his car on the circuit, the driver must try his best to remove his car from the track with the utmost caution as quickly as possible.
- 30.4 The Clerk of the Course may bring before the Panel of Stewards any unsporting behaviour on the part of a competitor or driver that is deemed contrary to the spirit of sport and fair play, when even the person concerned demands the literal application of the present regulations.
- 30.5 During the race:
- a) Should a towing device and/or lifting device break during the race, the track marshals will pull the car into a safe position using any part whatsoever of the chassis or the bodywork that they judge strong enough and the car may be excluded by the Panel of Stewards; The same applies when the car is in gear. In case of possible damage suffered by the car, protests will not be accepted and a penalty may be imposed by the Panel of Stewards.
 - b) Competitors will have no right to lodge protests if the car has been damaged as a result of the failure of any of the mandatory devices mentioned in (a).
- 30.6 All cars must compete within the track boundaries, this is governed by the white lines that define the track edge. The Stewards may impose the following penalties for non-respect of the track boundaries (limits):
- a) During Practice & Qualifying –
 - 1. Monetary fine, and/or
 - 2. Cancel the laptime set on the lap of the infringement.
 - b) During Race -
 - 3rd offence – Unsporting Flag
 - 5th offence – 30 sec time added penalty after race.

Art. 31 TYRES

- 31.1 The type and size of tyres supplied by panel of tyre suppliers appointed by the Organiser are to be used by all competitors during the event. Any changes of tyre supplier, type or size will be advised later via Additional Supplementary Regulations.

YHI (Malaysia) Sdn. Bhd. has been appointed as the only tyre supplier for the 2022 Malaysia Championship Series season with Yokohama is the sole nominated tyre for the Championship. Only tyres supplied and fitted by YHI (Malaysia) Sdn. Bhd. to the approved compounds (as approved for use) can be used at any Meeting.

The only Yokohama tyres permitted in 2022 are those specified below:

T – Production (TP) & :	Yokohama Advan A050 (M)
S – Production (SP20)	245/40R17 (TP, TP Turbo & SMax20)
	245/40R18 (TP, TP Turbo only)
M – Production (MTC):	Yokohama Advan Neova AD08R
	195/50R15 (MP & SMax16)

For safety reason (that may lead to unsafe release) during race, tyre supplier has the right to deny or refuse any tyre services 75 minutes before pit exit (of the respective category) opens.

Buffing or scraping tyres is not permitted at all time.

Art. 32 ADVERTISING

- 32.1 Competitors are obliged to carry the main sponsor and co-sponsor stickers (to be announced) at designated spaces on the car (advertisement location diagram is as per **Appendix IV**). The decision of the organisers is final. Stickers must be in position before a car can be scrutineered.
- 32.2 Any other advertising carried by a competitor must be declared to the organisers for approval. The organisers' decision on this matter is final.
- 32.3 The competition number supplied by the organisers cannot be mutilated. Any branding on them cannot be cut off. Tobacco advertising is strictly prohibited.
- 32.4 All forms of advertising prior to or after the race by competitors or their sponsors must obtain the approval of the organisers for correctness before publicity under pain of an advertisement of at least the same size and frequency in accordance with Part V: Article 11 of the NCR of the MAM. Approval for such advertising shall only be released at the earliest 24 hours after time of submission. The organisers shall not be held responsible for any delay in the approval of any advertisement.
- 32.5 Advertisements on the results shall comply with (24.4) above and carry the title of the event at a prominent place of the advertisement. The size of the title shall not be less than 1/10 of the size of the advertisement.
- 32.6 REJECTION FEE for not carrying sponsors decals is as follows:
- | | |
|---------------|------------------------|
| MAIN SPONSOR: | RM2,000 |
| CO-SPONSOR: | RM1,000 per co-sponsor |

The competition numbers and their background must be intact and not mutilated in anyway.

Art. 33 ORGANISER'S RIGHTS

- 33.1 The organisers may abandon, cancel or postpone the meeting or any part thereof. In this event, the competitor or entrant has no right to claim against the Organiser or Promoter in respect of any loss or expense he/she may thereby incur.
- 33.2 Cancel any class, which has less than 6 cars or amalgamate them to the next higher class at the discretion of the organisers.
- 33.3 Distribute the awards based on intermediate results at their discretion if through bona fide unforeseen circumstances, the competition is stopped before its completion.
- 33.4 Exclude any vehicle whose appearance, condition or performance is not of a standard appropriate for the competition.
- 33.5 Refuse an entry without giving any reason.
- 33.6 The Clerk of the Course of the meeting has the right to stop the race or any driver immediately due to any unavoidable circumstances or as the case may be.
- 33.7 The Organiser reserves the right for competition number 1, 2 and 3.

Art. 34 PROMOTER'S RIGHTS

All participants are prohibited from using caterers not registered with the Promoter.



MALAYSIA CHAMPIONSHIP SERIES 2022

C. TECHNICAL REGULATIONS

NOTE:

FIA Group N and Group A Cars must have current and valid homologation

Eligible cars must be mass-produced, Series Production Saloon cars, having at least 4 seats and in accordance with the dimensions defined by the FIA for Touring Cars.

Hybrid cars are defined by vehicle having two power sources to power the vehicle. The control system on these vehicles is free; however, the other mechanical parts of the car are subject to this regulation.

It should be clearly understood that if the following texts do not clearly specify that you can do it, you should work on the principle that you cannot.

READ IT, do not rely on memory. The clauses in the regulation are to be read as follows:

‘shall’, ‘must’ and ‘will’ indicate a compulsory requirement.

‘Should’ indicates a recommendation.

‘May’ indicates an option.

All parts of the car shall be identical to those used in the production model except where otherwise permitted by these regulations.

Standard: The word 'Standard' used within these technical regulations as a description of components is to be interpreted as 'The specified component from the manufacturers' or partners' original parts list for the model / engine shown on the entry form or registration form. No modifications permitted beyond the repair or adjustment processes specified by the manufacturer. Checking will be by comparison to spare parts supplied by the manufacturers official agent'.

Production: refers to original specification parts and components intended for a certain market and/or country. To avoid dispute, the competitors must clearly states in the vehicle specification sheet, the actual market and/or country for which the vehicle was intended.

Original Equipment/ Stock: refers to optional accessories made by the chassis manufacturer. The accessories must be mass produced and offered for sale to the general public.

STANDARD PART: Is a part, the specification, features, location and method of operation of a part are as provided when new by the motor manufacturer for the model and date of car.

STANDARD PATTERN PART: Replacement part that has a similar form shape and features as the standard part and is made using similar materials and manufacturing processes e.g. A standard part manufactured by a non-original equipment supplier that is fully interchangeable with the standard part.

Where clearly permitted in these regulations a part may be:

- (a) Re-worked, provided that the part remains identifiable as to its origin. The extent of the rework will only be as allowed by these regulations. Complete substitution is forbidden, or
- (b) Removed, or
- (c) Added, or
- (d) Substituted, by another of similar function but different performance characteristics. The limitations on the specification of the substituted part will be stated.

Where a part is defined as 'free' then it may be reworked, removed, added or substituted.

Rework of adjoining parts is only permitted where specifically allowed by these regulations.

It is permitted to repair a part, however the method of repair shall be such that the physical characteristics related to its function shall be the same as a new standard part.

Lightening of a component is allowed provided the component is non-structural and it is done in a neat and safe manner (e.g. no rough finishing etc). The scrutineers will have the right to ask a competitor to replace part(s) and/or component(s) that are deemed dangerous.

Threaded fasteners are free, provided the material type is not changed.

Titanium is prohibited for any parts.

The words "Variable Cam Engine" refers to engines, which have a device that alters the characteristic of the cam timing or advance. This may be actuated via hydraulic pressure or electrically.

CHASSIS: Chassis must be as per manufacturer's dimensions and design. VIN numbers will be checked to ensure the chassis is that of a standard car. VIN numbers must be clearly visible and readable by the Technical Director / Scrutineer.

It is the responsibility of the entrant to ensure that the car meets the criteria set forth in this technical regulation.

SECTION A

1) GENERAL

Art. 1 The Organiser reserves the right to amend the present regulations in agreement with the local ASN. The regulations are subject to change, which will be published via Additional Supplementary Regulations.

Art. 2 To be eligible, all cars must comply with the prescriptions of the present technical specifications. Any unauthorised modification is strictly prohibited.

Art. 3 Only the organiser decides on the admission of a car and the decision taken is final. This point is explicit and completely accepted by all the competitors and drivers by submitting their entry. Any protest against the classification of admission of a car following the organiser's decision is consequently not possible.

2) ELIGIBLE CATEGORIES

A) Categories eligible for the 2022 Malaysia Championship Series are as follows:

2.1 T – Production (TP)

2.2 M – Production (MTC)

2.3 S – Production (SMax 20)

2.4 S – Production (SMax 16)

3) STANDARD TECHNICAL REQUIREMENTS

1. GENERAL

The provisions of Art. 253 for Group N of Appendix J of the FIA International Sporting Code apply in full.

2. ADDITIONAL FASTENERS

Two additional safety fasteners must be fitted for each of the bonnet and boot lids. The original locking mechanisms must be rendered inoperative or removed.

3. TOWING DEVICE

All cars will be equipped with a rear and front towing device for all events. It will be clearly visible, marked and painted in yellow, red or orange.

4. DRIVER'S SEAT

The original driver's seat must be replaced by an FIA-homologated competition bucket seat (8855/1999 standard) with five (5) passages for the safety harness straps.

The original seat mountings may be removed. This is not considered as a structural part. The use of the competition seat mountings homologated with the bucket seat is recommended. Hans compatible seats are recommended.

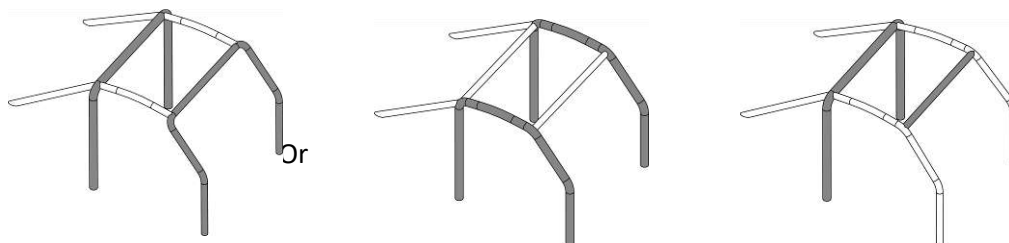
5. SAFETY HARNESS

A safety harness equipped with a turn buckle release system and having a minimum of five (5) anchorage points, homologated by the FIA in accordance with Article 253.6 of Appendix J, is compulsory. Hans compatible harness are recommended.

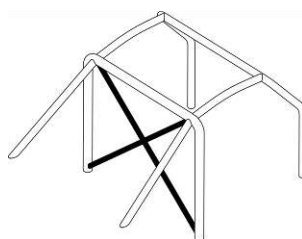
6. ROLLOVER STRUCTURES

A recommended rollcage complying with Article 253.8 of Appendix J. Minimum requirement Art 8.3.1, Art 8.3.2.1.1, Art 8.3.2.1.2 and Art 8.3.2.1.3.

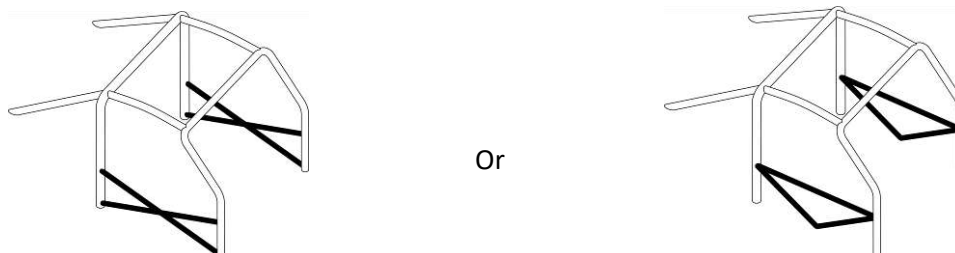
Main Cage



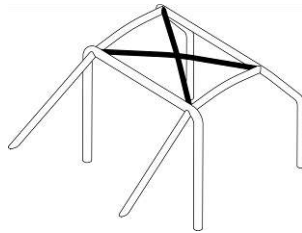
+ Diagonal Member



+ Door Bars



+ Roof Reinforcement



Roll cage mounting points may be welded or bolted to the body. They may be attached to the boot and rear wheel arches. Minimum number of points must be 6 for touring cars. No inferior quality tubing will be accepted.

Should removable members be used in the construction of a safety cage, the dismountable joints used must comply with a type under FIA Article 253 (Drawings 253-37 to 253-47).

7. EXTINGUISHERS – EXTINGUISHING SYSTEMS

Automatic extinguishers homologated in accordance with Article 253.7.2 of Appendix J are recommended. During events, all plumbed-in extinguisher systems must be in an 'Armed' condition (ie be capable of being operated without the removal of any safety device) at all times whilst taking part in practice sessions or races including until released from parc ferme. (Manual should be armed).

8. HEAD AND NECK SUPPORT (HANS)

Head and Neck Supports (HANS) safety device is compulsory for all categories.

9. HELMETS (FIA Appendix L Chapter III Art. 1)

All drivers must wear crash helmets which meet one of the standards listed in FIA technical list N° 25 of Appendix J.

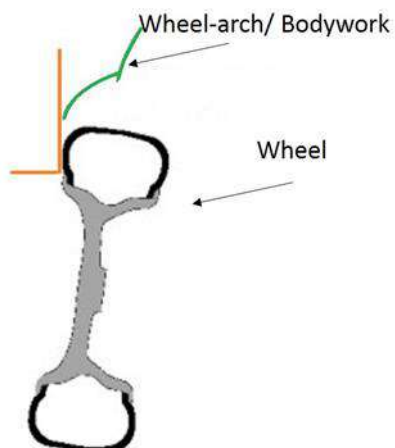
Only helmets approved in accordance with FIA standard 8858 (Technical List N°41), 8860 (Technical List N°33) or 8859 (Technical List N°49) are authorised.

No helmet may be modified from its specification as manufactured, except in compliance with instructions approved by the manufacturer and one of the FIA listed standards organisations, which certified the model concerned.

Any other modification will render the helmet unacceptable for the requirements of the present article.

10. WHEEL VISIBILITY

The upper part of the tyre, down to the flange over the wheel must be within the perimeter of the wheel-arch/ bodywork when viewed vertically from above. Kindly refer to the respective article(s) of each category/ class for the specification/ dimension.



11. CHECKING TOOLS

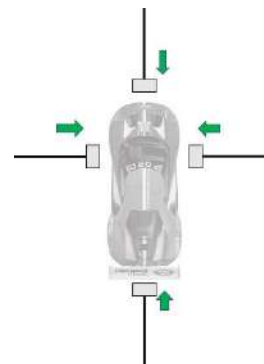
The organiser will have a stock of equipment/ tools at event(s) for use in eligibility checks on the following components:

Ride Heights
Vehicle Weight
Bore & Stroke

Ride height (Ground Clearance Measurement)

Measuring location:

The measurement will be conducted in a designated area during technical scrutineering. The measuring tools are available to the participating teams to check the minimum ground clearance after consultation with the technical director.



SECTION B - T-PRODUCTION (TOURING)

SPECIFIC MODIFICATION (UNDER ARTICLE 277 OF FIA APPENDIX J) VALID FOR T-PRODUCTION CAR

(Other specification not mentioned, please refer to respective articles of FIA at www.fia.com)

Permitted Vehicles

FIA Article 254 : Group N Cars above 1900cc
FIA Article 255 : Group A cars above 1900cc
FIA Article 277 : Non FIA Homologated Touring Cars Above 1900cc (2500 minimum annual production)

Engine capacity for any Two Wheel Drive (2WD) vehicle, whether Front Wheel Drive (FWD) or Rear Wheel Drive (RWD). Entries must able to produce document(s) should any conversion from 4-wheel drive to 2-wheel drive is made and it must be remained for the rest of the season. It is NOT permitted to convert a front wheel drive car to rear wheel drive or vice versa.

All family brand of mass production engine, transmission, suspension and body parts are allowed.

T1. MINIMUM WEIGHT

The T-Production cars are based on production models and are fundamentally different from each other. To create a more level playing field for competition, the organiser has the ability to apply fine adjustments in the performance of individual makes to allow each model to be competitive.

A calculation tool allows the targets for balancing the different car models to be met, without human interpretation or decision-making, using mathematical formulas based on the quantified, public, specific, measured data obtained during each race.

The following modifications may be applied:

- i) Minimum weight of the car
- ii) Restrictor
- iii) Any other technical modification that the organiser may deem necessary.

The following minimum Car dry weight (excluding Driver and fuel) must be respected at all times during the Event.

General Model

1901cc up to 2000cc	1,100 kg
Turbo cars	1,150 kg

Specific Model (with BOP)

Make	Model	BOP Ballast Weight (kg)	Total min. Racing Weight (kg)
Toyota	86	-10	1050

•**Supercharged/ Turbocharged engines**

The nominal cylinder capacity is limited to 2000 cm³ maximum.

The supercharged / turbocharged unit aftermarket is allowed.

The weight of any car may be checked at any time during the Events. The minimum qualifying and finishing from race(s) weight for cars shall be the specific weight of car plus success/ penalty weight ignoring the fuel factor. These weights are official for whatever weighing taken when the driver is not present.

If a car loses a part during a Qualifying Session or a Race, the weight of this part may be taken into account during weighing at the discretion of the Clerk of the Course, following consultation with the Chief Scrutineer.

T2. INTERIOR

T2.1 The steering wheel is free.

T2.2 The front seat may be changed for a racing type in order to use 4-point seat belt, minimum. All other seats may be removed.

T2.3 The air-conditioner and radio equipment (including wiring) may be removed.

T2.4 All carpets and sound proofing material and interior trim may be removed.

T3. ENGINE

The Engine as supplied in the respective model may be modified, provided that the original intake manifold and throttle body or carburettors are used. These may be freely modified internal turbo/supercharged and will be rated according to FIA coefficients. The Scrutineers reserve the right to seal any engine they see fit to do so. Any such seal must only be broken by the Scrutineers, breach of this will be penalised with exclusion from the Race.

T3.1 CYLINDER HEAD

T3.1.1 The cylinder head may be ported and polished. The compression ratio may be adjusted for optimum running on fuel sold by the Organiser.

T3.1.2 The camshaft and cam timing is free.

T3.1.3 The cylinder head gasket is free.

T3.2 IGNITION

T3.2.1 The spark plugs/igniters (glow plugs) are free.

T3.2.2 The high tension plug wires are free.

T3.2.3 Modification to advance the ignition curve of the distributor is permitted provided the distributor is the original part.

T3.3 COOLING SYSTEM

- T3.3.1** The thermostat is free or may be removed. The control system at which the fan cuts in is free. All water bypass hoses on the engine block and cylinder head may be sealed off.
- T3.3.2** Larger radiator and/or Intercooler is permitted provided it fits in the original location without having to cut the bodywork.
- T3.3.3** Oil coolers may be fitted. Metal braided hoses must be used to avoid bursting in aftermarket oil coolers.
- T3.3.4** The pipes between the supercharging/ turbocharging device, the intercooler and the manifold are free but their only function must be channel air.
- T3.3.5** The liquid cooling lines external to the engine block and their accessories are free. Lines of a different material and/ or diameter may be used.
- T3.3.6** The radiator fans are free. Any water spraying system is prohibited.
- T3.3.7** All air openings must have the sole effect of inducing the necessary air for the cooling of the radiator, and must not have any aerodynamic effect.

T3.4 INDUCTION SYSTEM

- T3.4.1** The original fuel injection system must be retained but may be modified. Air filters are free.
- T3.4.2** The inlet manifold may be internally modified.
- T3.4.3** Injection nozzles are free.
- T3.4.4** Fuel pressure regulators are free.
- T3.4.5** Additional fuel containers to avoid fuel starvation at turns are permitted.

T3.5 LUBRICANTS

- T3.5.1** Choice of lubricant is free.
- T3.5.2** Replacement oil filters of any brand are permitted.
- T3.5.3** The oil sump may be baffled.

T3.6 EXHAUST

The exhaust system is free provided it does not protrude outside the perimeter of the Car.

T3.7 TACHOMETERS

Tachometers may be fitted to cars that are not fitted with one as standard.

T3.8 TELEMETRY

The use of telemetry is forbidden

T4. SUSPENSION

- T4.1** The suspension may be modified provided that the system and mounting point locations are not changed.
- T4.2** No part of the Car (with exception of tyres) must touch flat ground when 2 tyres on the same side of the Car are deflated.
- T4.3** MacPherson struts and shock absorbers may vary in size and brand.
- T4.4** Altering the front camber with adjustable top mounts is permitted even though this item is also part of the steering system. Camber and castor settings are free.
- T4.5** Adjustable spring cups are permitted.
- T4.6** Fitting of transversal strut to the top absorber mounting is permitted. This must not alter the mounting points.
- T4.7** Rose joints are permitted.

T5. BRAKING SYSTEM

- T5.1** Front braking system is free provided they are mounted on the fixation points of the original brakes and they are comply with the following prescriptions:
- (i) The master cylinder(s) is(are) free
 - (ii) The maximum number of pistons per wheel is 6 (six)
 - (iii) The diameter of brake disc is 380mm
 - (iv) Brake disc must be made of from ferrous metallic material
 - (v) Rear brake callipers may be replaced but must not have more than four (4) pistons per wheel
- T5.2** ABS may be disconnected or removed.
- T5.3** Brake hoses and fluids are free. Brake pads and shoes material are free.
- T5.4** Brake dust cover may be altered in shape or removed.
- T5.5** Cooling ducts, from the bumper only or beneath the floor for rear brakes leading to the brakes only are permitted.

T6. GEARBOX AND CLUTCH

- T6.1** Mechanical limited slip differentials are permitted.

- T6.2** Clutch plate, pressure plate and flywheel are free.
- T6.3** Gear ratios are free provided their numbers are not increased or decreased. Straight cut gears are allowed. Gearbox oil coolers are permitted.
- T6.4** If a sequential box is used, the official minimum weight of the car will have an additional 30kg added.

T7. WHEELS AND TYRES

- T7.1** The upper part of the tyre, down to the flange over the wheel must be within the perimeter of the wheel-arch/ bodywork when viewed vertically from above. The wheel diameter may be increased or decreased from original specifications provided they fit in the original wheel arches.
- T7.2** Hub caps on standard wheels must be removed.
- T7.3** The dimensions of the 4 rims + flanges are 9.5" x 17" and 9.5" x 18" with maximum tyre width 245mm. Refer to section **B. Sporting Regulations Art. 31**
- T7.4** The tyre supplied by the tire supplier appointed by the Organiser MUST be used by all competitors during the event. Refer to section **B. Sporting Regulations Art. 31**

T8. SAFETY EQUIPMENT/BODYWORK

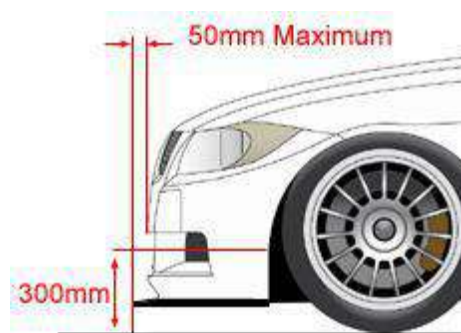
- T8.1** (i) Cars must be fitted with laminated windscreens. Perspex or Lexan may be used for other glass areas. Securing rivets may be used. Air induction to the driver's window is permitted.
- (ii) It is allowed to have net window covering the driver's door window opening forward till the centre of steering wheel if item T8.1 (i) above does not apply. Details:
- (a) The window net must cover the opening forward to the centre of the steering wheel and be able to withstand any load applied at any point.
 - (b) The net may be locally modified to preserve the driver's view of the external mirror.
 - (c) The net must be affixed by means of a rapid release system so that, even with the automobile inverted it must be possible to detach the mechanism with one hand.
- T8.2** All Cars must be fitted with a cut-off switch to stop all electrical supply to the engine and fuel system.
- T8.3** The cut-off switch must be able to operate from inside and outside and must be located in front of the driver's side of the car. The outside switch must be marked by a red spark in a white edged, blue triangle with a base of a least 12cm.
- T8.4** The body may be painted or wrapped in any colour scheme.
- T8.5** For the avoidance of doubt the transversal cross bar fitted to the roll cage will not be treated as reinforcement to suspension mounting points.

- T8.6** Extra welding to parts of the bodywork is permitted. However, the use of strengthening plate on the original body panels and suspension points is prohibited.
- T8.7** All lights must be taped diagonally across with good quality clear cellophane tape at least 1 inch (2.5cm) wide.
- T8.8** The placement of taillights must be of the original car.
- T8.9** All cars in Touring Production class MUST be fixed with cross door-bar for safety.

T9. AERODYNAMIC DEVICES

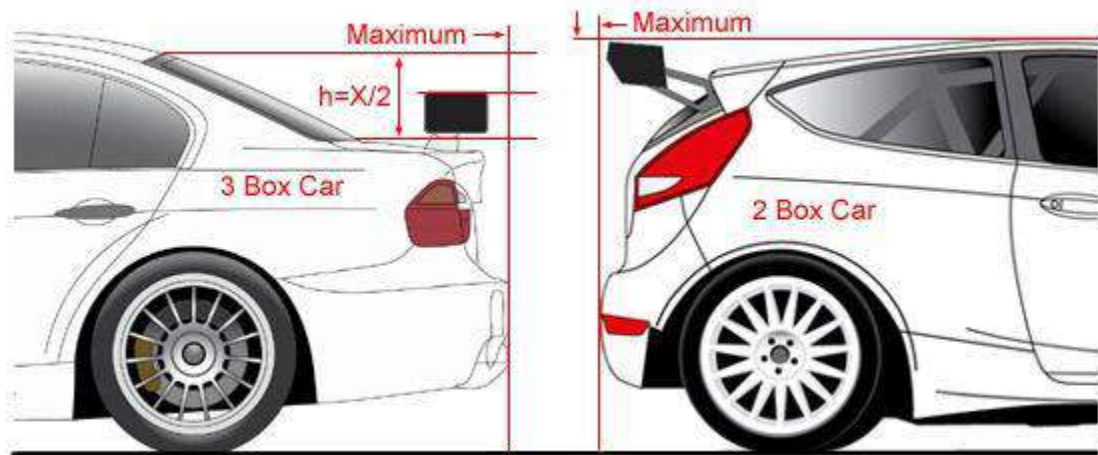
- T9.1** Other than those allowed in this Regulation, only those aerodynamic devices which are permanently mounted on the bodywork and which are Series Production parts on the 2,500 units produced for normal road use in the country of origin are permitted.
- T9.2** It is permitted to fit a spoiler or air dam on the front of the car such that no part of it shall extend more than 50mm overall width of the bodywork, measured perpendicular to the coachwork at any measured point and it shall be no wider than the front flares/mudguards. A front diffuser or an air dam under-tray may be installed. No part of the front diffuser or under-tray shall extend further rearward than the vertical centreline of the front wheel hubs. Each part of the diffuser or under-tray shall be within the vertical projection of the car, including any modified coachwork. No dive planes allowed.

Additional wheel arch is allowed with total maximum of 30mm. The areas that are defined as front spoilers are the front bumper area before the opening of the wheelhouse measuring not more than 300mm from level ground. The size and the dimension of the apertures on the original front bumper must be respected. (See technical drawings).



- T9.3** It is permitted to fit a rear spoiler provided that it complies with the following:
 - a. For a three-box car, the spoiler must be mounted on the boot. The position of the spoiler must not be more than 50% the height of the rear screen.
 - b. For a two-box car, the spoiler must be mounted on the rear hatch/boot lid. The position of the spoiler must not protrude above the highest point of the car when viewed from the side.
 - c. The spoilers must not protrude outside the perimeter of the bodywork.

- d. For a non-original equipment rear spoiler, the spoilers must have a zero angle between frontal part and rear area of the wing profile (see technical drawings).



T10. FUEL

Fuel sold at the Circuit and/ or **racing fuel** comply with FIA article 252.9 can be used for the event.

Only the originally equipped fuel tank or an FIA FT3 1999, FT3.5 or FT5 or Organisers approved tank that complies with FIA (FT3 1999, FT3.5 or FT5) is allowed. An add-on fuel cell (FT3 1999, FT3.5 or FT5 or organiser's approved) on the top of the main fuel cell and installed in a housing corresponding to this regulation may be used up to the total maximal fuel volume. The total maximum capacity of the tank(s) is 80 litres.

T-PRODUCTION - TURBO CLASS

TC1. Other than the modifications permitted expressly below, **MCS Technical Regulations 2022** and **SECTION B T-Production** should be followed. Anything which is not expressly authorized by the present Regulations is forbidden.

TC2. Engine

2.1 Turbo charged mass production engines without modifications. Naturally aspirated engine must remain a naturally aspirated engine and turbocharger engine must remain a turbocharger engine. The addition of a supercharger not complying with the original system is consequently not eligible.

2.2 Chassis and engine must be from end production of 2015 and above.

2.3 No nitrous oxide systems are allowed

TC3. Exhaust System

3.1 Free after Manufacturer Turbo down pipe

- 3.2** The exit of the exhaust may be on the right or left side of the car, behind the mid-point of the wheelbase and below the door sill. No exhaust pipe may protrude beyond the perimeter of the car's bodywork.

TC4 Differential

The use of mechanical type Limited Slip Differential is authorized provided that it can be fitted in the series housing without any modification.

TC5 Braking System

- 5.1** In accordance with Article T.5 Braking System (T - Production)
- 5.2** Brake Pads are Free but original shape and Dimension to be maintained.
- 5.3** Brake Fluid Free Brake Hoses Braided Hoses are allowed.

TC6 Suspension

- 6.1** Shock absorbers and Springs free, External shock canisters allowed.
- 6.2** The reinforcing of the structural parts of the suspension (with the exception of anti-roll bars) and its anchorage points by the addition of material is allowed.
- 6.3** Bushings and Mountings should conform to original design and size, uprated material is allowed.

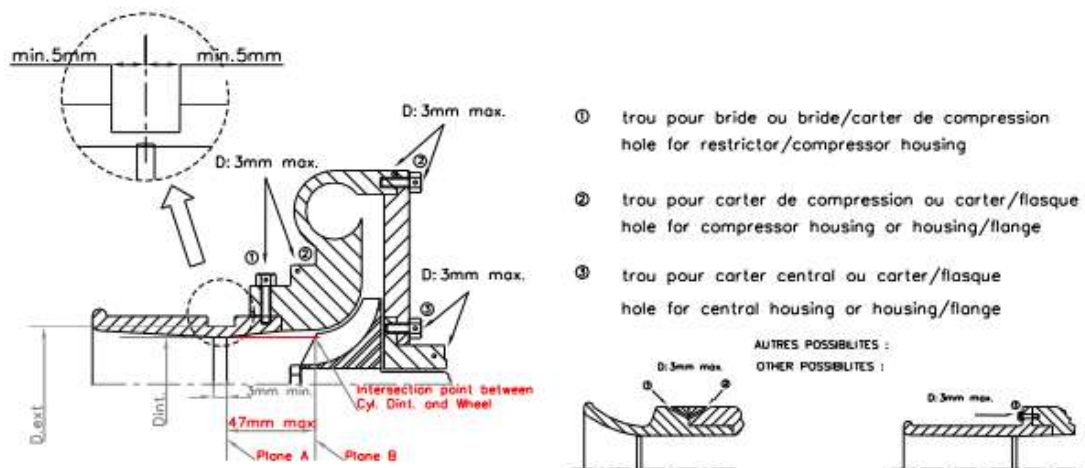
TC7 Restrictor

All the air necessary for feeding the engine must pass through this restrictor, which must respect the following: See Drawing

- a. The maximum internal diameter of the restrictor is (increase/ decrease of 1.5mm for every 100cc difference):-
- | | |
|-------|---------|
| 2.0cc | 30.0mm. |
| 1.8cc | 33.0mm. |
| 1.6cc | 36.0mm. |
| 1.5cc | 37.5mm. |
| 1.4cc | 39.0mm. |
- b. This diameter must be maintained for a minimum length of 3 mm.
- c. This length is measured upstream of plane A.
- d. Plane A is perpendicular to the rotational axis of the turbocharger and is at a maximum of 47 mm upstream of plane B, measured along the neutral axis of the intake duct.
- e. Plane B passes through the intersection between the most upstream extremities of the wheel blades and a cylinder of 45 mm diameter the centreline of which is the rotational axis of the turbocharger.

This diameter must be complied with, regardless of the temperature conditions.

The external diameter of the restrictor at its narrowest point must be less than 51 mm and must be maintained over a distance of 5 mm to each side.



The mounting of the restrictor onto the turbocharger must be carried out in such a way that two screws have to be entirely removed from the body of the compressor, or from the restrictor, in order to detach the restrictor from the compressor.

Attachment by means of a needle screw is not authorised.

For the installation of this restrictor, it is permitted to remove material from the compressor housing, and to add it, for the sole purpose of attaching the restrictor onto the compressor housing.

Local modifications (machining and/or addition of a sleeve/connector) to the outside of the compression housing are permitted at the level of the air outlet in order to improve the connection with the charge pipe.

The heads of the screws must be pierced so that they can be sealed.

The restrictor must be made from a single material and may be pierced solely for the purpose of mounting and sealing, which must be carried out between the mounting screws, between the restrictor (or the restrictor/compressor housing attachment), the compressor housing (or the housing/flange attachment) and the turbine housing (or the housing/flange attachment) (see Drawing).

SECTION C - M-PRODUCTION (MODIFIED)

SPECIFIC MODIFICATION (UNDER ARTICLE 277 OF FIA APPENDIX J) VALID FOR M-PRODUCTION CAR

(The regulations are subject to change, which will be published via Additional Supplementary Regulations – ASR)

1) GENERAL SPECIFICATION

Eligible Cars are those cars of which at least 2,500 identical units must have been produced in 12 consecutive months and which have been available for sale. The model must be for public sale or produced in the year 2007 and after. The organizer will have the right to grant special dispensation for cars which do not fit the above criteria. To ensure stability and to safeguard the investment of the competitors, the minimum production date will be pegged at the year 2006 until the year 2019 in the Gregorian calendar.

Only cars with two-wheel drive (Front Wheel or Rear Wheel Drive) are eligible. Cars with forced induction are not eligible.

Permitted Vehicles

FIA Article 254 : Group N Cars 1401cc to 1600cc

FIA Article 255 : Group A Cars 1401cc to 1600cc

FIA Article 277 : National Series Production Cars 1401cc to 1600cc
(2,500 units minimum production)

2) SPECIFIC MODIFICATIONS

M1. SEALING

Engines must have two bolts on the camshaft covers drilled with 3mm holes for the purpose of sealing. Similarly, two bolts on the oil sump pan must also be drilled with 3mm holes. It is the competitor's sole responsibility to ensure that all metal seals and any additional paint seals, are kept in a clean and unbroken condition. Only the scrutineer can give permission for seals to be broken and / or removed.

M2. MINIMUM WEIGHT

The minimum weight of the car with all fluids at as race level and excluding the driver is 1000kg. This is measured with a maximum of 3 litres of fuel.

These minimum weights must be respected at all times during the event, in particular when the car crosses the finish line. It is permitted to complete the weight of the car by one or several ballast, provided that they are strong and unitary blocks, fixed by means of tools with the possibility of affixing seals, and placed on the floor of the cockpit or the luggage compartment, visible and sealed by the Scrutineers. The ballast must be attached to the shell/chassis via 8.8 class bolts, with a minimum diameter of 8 mm, and counterplates, according to the drawing below. The minimum area of contact between shell/chassis and counterplate is 40 cm² for each fixing point. The ballast must not have sharp edges. The scrutineer would have the rights to reject any ballast which is deemed unsafe.

If a car loses a part during a Qualifying Session or a Race, the weight of this part may be taken into account during weighing at the discretion of the Stewards, following consultation with the Technical Delegate/ Technical Director.

M3. ENGINE

M3.1 GENERAL

Any four stroke piston engine with the cylinder capacity not exceeding respective class limit can be freely chosen from the manufacturer range of engine provided that the said engine must be in production as of 1st January 2007. The position of the engine must be in accordance to the original layout from the original model that the engine came from.

Replacement engine and transmission mounts are permitted. The number and attachment points to the bodysell must remain as standard production (e.g. 3 mountings, 1 located on the sub-frame, 1 located on the left chassis beam and 1 located on the right chassis beam). The position of the mounting may be moved within this boundary.

Where a minimum component weight is specified, this must be respected unless the production component is lighter than the specified minimum. Should a production component be chosen, this component must not be altered or modified in any way.

The organizer may at its sole discretion approve base engines of a different capacity.

M3.2 MAXIMUM ENGINE CAPACITY

The volume generated in the cylinders as defined as:

$V = 0.7854 \times \text{Bore}^2 \times \text{Stroke} \times \text{No. of Cylinders}$

The results would be rounded up to 2 decimal places.

The maximum capacity is 1600cc with tolerance of 0.5% allowable.

M3.3 CYLINDER HEAD

The material and thickness of the cylinder head gasket are free. The cylinder head may be adjusted by planning. The valve springs and their retainers are free but the springs must be made of steel and the retainers made of an identical material to the original. The intake and outlet ports in the cylinder head, as well as the ports in the intake manifold, may be machined and modified however the respective ports flow and purpose may not be changed; reverse direction cylinder heads are not allowed. The valve seats are free, as are the valve guides, but the respective angles of the valve axes must be retained. Compression ratio is free.

M3.4 CAMSHAFT

The pulleys for driving the camshaft are free (**subject to 3.12**) provided that the original timing belts and/or chains are used. The camshaft(s) are free. The number and diameter of the bearings must remain unchanged. Variable valve timing systems may be removed.

M3.5 VALVES

The material must be steel. The diameters of the stem and the head of the valves must be the same size as stock valves (standard part).

M3.6 CYLINDER BLOCK

May be planed parallel to the original face for the purpose of increasing the compression ratio. The cylinder bores may also be increased to achieve the maximum cylinder capacity of the permitted class. Ceramic and nikasil sleeve is prohibited. Main bearing caps maybe replaced and/or strengthened by additional straps and/or additional bolts.

M3.7 PISTONS AND RINGS

Free, provided that:

- a) The piston is manufactured from the same type of material i.e aluminium, as the production part.
- b) The piston must have at least 3 rings – 1 oil control and 2 compression rings.

M3.8 CRANKSHAFT

Must be standard part production.

M3.9 CONNECTING ROD

Free, provided that the material must be steel.

M3.10 ENGINE BEARINGS

Must be standard however service is allowable limited to 0.25mm.

M3.11 FLYWHEEL

Free provided that the material is the same as the original flywheel. Minimum weight: 4000 grams

M3.12 ENGINE PULLEYS

Must be made from metal unless different material comes from standard production

M3.13 TELEMETRY

The use of telemetry is forbidden

M4. AIR INTAKE SYSTEM

M4.1 IDLE UP AND COLD RUNNING ENRICHMENT COMPONENTS AND EXHAUST GAS RECIRCULATOR

May be removed, provided that any aperture downstream of the throttle body(s), created by their removal, is plugged.

M4.2 INTAKE MANIFOLD

- a) The intake manifold is **standard** with a diameter of 60mm at the throttle valve opening. The single-valve unit is free but the operating principal must be of a butterfly valve. The thickness of the throttle valve must be constant. The use of composite material is authorised, provided that it is fire-retardant. Electronic throttle valve maybe changed to mechanical mechanism. Variable geometry intake manifold is prohibited.
- b) If the production intake manifold includes a variable geometry mechanism, this can be retained. (subject to 4.3).

M4.3 THROTTLE BODY

One air intake only with a max. diameter of 60mm is authorized. The single-valve unit is free but the operating principal must be of a butterfly valve. Electronic throttle body maybe changed to mechanical mechanism.

M4.4 AIR FILTER

The air lines upstream of the air filter box are free and the air lines downstream of the air filter box towards the throttles are free. The air filter box is free under the following conditions:

- a) There must be a filtering cartridge in the box. This cartridge is free as long as it filters the dust particles. All the air admitted to the engine must pass through this air filter.
- b) Only one air outlet from the filter to the throttle body. Engine breather from the engine to the intake must be rendered inoperative and must be completely blocked.
- c) The use of fiber glass and/or composite material is authorized provided that it is fire-resistant. The position of installation of the air filter box in the engine compartment is free.
- d) K&N type filter units do not require a filter box

M5. FUEL & IGNITION SYSTEMS

Spark Plugs, high tension cables, ignition coils are free. **Only fuel sold at the Circuit is to be used for the event.**

M5.1 FUEL INJECTORS

- i) Any commercially available fuel injectors may be used but the number is limited to one injector per cylinder.
- ii) Fuel regulators and fuel rail are free
- iii) Engine Control Unit (ECU) are free

M6. EXHAUST SYSTEMS

The exhaust system is free from the exit of the cylinder. The exhaust tail pipe must exit at the rear of the vehicle.

The section of the exhaust silencers or of the catalyst itself must always be round or oval.

M7. LUBRICATION SYSTEMS**M7.1 CATCH TANK**

(Oil/Air Separator): Compulsory. Minimum capacity; 2000cc.

M7.2 CAM COVER(S)

May be internally baffled to inhibit oil being dispersed into breather hose(s). Breather hose adapter fitting are free. However, the outside appearance and dimensions of the rocker cover(s) shall remain original.

M7.3 LUBRICATION OIL WAY/GALLEY

May be enlarged or restricted.

M7.4 OIL SUMP

The original sump may be reworked to:

- a) Increase the oil capacity
- b) To fit internal baffles, and/or gates.

M7.5 OIL PUMP

The original oil pump may be reworked and modified to:

- a) Increase its capacity provided that it remains in the original location.
- b) The discharge valve spring may be substituted to vary the oil pressure.
- c) The oil pickup may be reworked or substituted.

M7.6 ENGINE OIL COOLERS AND OIL COOLER DUCTING

Free, provided that:

- a) They are located within the body work, and
- b) The ducting serves only to deliver air to the oil cooler.

M8. COOLING SYSTEM

M8.1 Radiator, ducting, header tank, fans and thermostat are free.

M8.2 WATER PUMP

Must retain original standard, but the water pump drive pulley is free.

M9. TRANSMISSION

M9.1 GENERAL

- i) If the series production gearbox for the respective model has more than five (5) forward gears, the gear wheels as from the sixth (6th) forward gears can be rendered operative.
- ii) The gears and the method of engagement (syncromesh or dog clutch) is free.
- iii) The interior of the gearbox is free.
- iii) An alternative sequential gearbox with maximum 5 gears may be fitted in replacement to the original gearbox. A 30kg additional weight penalty will be applied for all cars fitted with a sequential gearbox. Paddle shifts are permitted.
- v) LSDs are permitted

M9.2 GEAR RATIOS

Gear ratios are free. However entry/entrance need to submit their gearbox ratio and final drive as a declaration.

M9.3 GEAR SELECTION

Modifications to the bodywork for the mounting and passage of any new gearshift control are authorised only if they are not at variance with other points of these regulations.

M9.4 DIFFERENTIAL

Free. Electronically and hydraulically control differential are not allowed.

M9.5 TRACTION CONTROL/ LAUNCH CONTROL SYSTEMS

Only Factory OEM control systems are allowed. All forms aftermarket or motorsport of traction control and launch control are prohibited. All sensors on the wheels, drive shafts and differential are prohibited.

M9.6 CLUTCH

Size and the number of plates are free. The friction disc(s) must not be made from carbon. The clutch must be fixed to the engine flywheel. The original location of the clutch must be retained.

M9.7 TRANSMISSION COOLER

Additional oil radiators, as well as a system for circulating the oil are authorised.

M10. SUSPENSION

M10.1 FRONT RUNNING GEAR

Limitations and modifications allowed:

- a) The joints may be of a different material from the original ones (e.g. harder silent blocks, aluminium, Uniball joints, etc.).
- b) It is permitted to move the suspension pick-up points within a radius of 20mm from the original points.
- c) The original suspension part may be modified to allow adjustments to the camber/caster angle.
- d) The steering rods, the steering joints and their connecting parts are free but must be made from ferrous material.
- e) The upper joints of McPherson suspension parts of the front running gear are free provided that the original mounting points, on the bodyshell side, are retained. Adjustable upper mounts on the struts are permitted but this must not allow adjustments of more than 20 mm in relation to the original articulation point.

M10.2 POWER-STEERING

The driving pulley of a hydraulic power-steering pump is free. A hydraulic power-steering pump may be replaced with an electric power-steering pump, provided that this electric pump is fitted on any series vehicle and is commonly on sale.

M10.3 REAR RUNNING GEAR

Limitations and modifications allowed:

- a) The joints may be of a different material from the original ones (e.g. harder silent blocks, aluminium, Uniball joints, etc.).
- b) The original suspension part may be modified to allow adjustments to the camber/toe angle.
- c) It is permitted to move the suspension pick-up points within a radius of 20mm from the original points.

M10.4 SINGLE-LINK REAR SUSPENSION/BEAM AXLE

The limitations and modifications for a single link/beam axle type rear suspension system are as follows:

- a) The original suspension parts may be modified in order to allow the adjustment of the camber and the toe. The addition of material is allowed.
- b) The combination and the standard fitting of the spring and of the shock absorber may be modified.
- c) It is permitted to move the suspension pick-up points within a radius of 20mm from the original points.

M10.5 REAR RUNNING GEAR – GENERAL

Any other modifications to the bodyshell, apart from those modifications authorised to the rear running gear, are prohibited.

M10.6 OTHER PROVISIONS

The geometry of the running gear is free within the limits of the original adjustment possibilities set out in these regulations.

M10.7 STABILIZERS/ANTI-ROLL BARS

The original anti-roll bars and their links may be replaced by anti-roll bars and links of free design. The operating principle must be solely mechanical. The anti-roll bars and their links must be made from metallic material and must not be adjustable from the cockpit.

M10.8 TRACK

The tracks are free. Track extenders may be used if they are immovably attached to the wheel hubs. The wheels above the hub centre line must be covered by the bodywork

M10.9 REINFORCEMENTS

Strengthening of the suspension parts and the suspension mounting points through the addition of material is allowed provided that the material used follows the shape of the original part and is in contact with it. The suspension reinforcements must not create hollow sections and must not allow two separate parts to be joined together to form one.

M10.10 WHEEL BEARING

The wheel bearings may be replaced by strengthened bearings of the same type and same diameter as the original ones.

M10.11 SILENT BLOCK – ARTICULATION

The silent blocks for the mounting of the cradles/subframes and/or the cross members may be of a different material from the original (e.g. harder silent blocks, aluminium, nylon rings) as long as the position of the cradle/subframes and/or cross members in relation to the bodyshell remains identical to that of the original following the three (3) axes of reference. The cradles/subframe and/or cross members, the bodyshell and the original mounting points may in no way be modified by this action.

M10.12 SUSPENSION TRAVEL LIMITER

A strap or cable for limiting the suspension travel may be affixed to each suspension. To this end, holes of a maximum diameter of 8.5 mm may be bored on the bodyshell side and on the suspension side.

M10.13 SPRINGS

(i) COIL SPRINGS

Coil springs are free, provided that they fulfil the following conditions:

- a) Their number is free, provided that they are mounted in line with one another and that their type corresponds to the original type of spring.

- b) The shape, dimensions and material of the spring seats are free.
- c) The spring seats may be made adjustable if the adjustable part forms part of the seats and is distinct from the other original parts of the suspension and the chassis (it may be removed).

(ii) LEAF SPRINGS

The length, width, thickness and vertical curve are free.

(iii) TORSION BARS

Torsion bars may be replaced but the replacements must be made from steel. For vehicles with torsion bars, coil springs may be added on the axle concerned, provided that they are concentric to the shock absorbers.

M10.14 MISCELLANEOUS

Parts for preventing the springs from moving in relation to their mounting points are authorised.

M10.15 SHOCK ABSORBERS

Free, provided that their number, their type (telescopic, arm, etc.), their working principle (hydraulic, friction, mixed, etc.) and their attachment points remain unchanged.

In the case of an oil-pneumatic suspension, the spheres may be changed as regards their dimension, shape and material, but not their number. A tap, adjustable from the outside of the car, may be fitted on the spheres.

M10.16 REINFORCEMENT BARS

- (i) Reinforcement bars may be fitted on the suspension mounting points to the bodyshell or chassis of the same axle.
- (ii) For the fixation of a transversal strut between two upper points of the bodyshell, a maximum of three (3) holes on each side, of a maximum diameter of 10.5 mm, will be authorised. The mounting rings of the upper transversal struts may be welded to the bodyshell. Apart from these points, the upper bar must not be mounted on the bodyshell or the mechanical parts.

M11. BRAKES

M11.1 GENERAL

All parts of the brake callipers must be made from aluminium materials with a modulus of elasticity no greater than 75 GPa. The internal parts of the brake callipers may be made from steel or titanium.

The following prescriptions apply to the complete braking system:

- a) The brake lines and their fitting method are free;
- b) The original handbrake may be removed or replaced by a hydraulic valve operated manually and without any intermediate system by the driver;
- c) If, in its original version, a car is equipped with servo brakes, this device may be disconnected or removed.
- d) If, in its original version, a car is equipped with an anti-lock braking system, the control unit may be removed.
- e) The location of the brake lines is free provided that the prescriptions of Article a. 253-4 of Appendix J are respected. The brake lines may be replaced by aircraft-quality lines. The connection of the dual braking circuit is free.
- f) Original apertures in the bodywork, e.g. for fog lamps, may be used to bring the cooling air to the brakes; the connection of the air lines to the original apertures in the bodywork is free provided that these apertures remain unchanged; if these apertures are not required they may be closed 10mm behind the external surface
- g) If the car does not have any original apertures, two (2) circular apertures of a maximum diameter of 10 cm may be made in the front bumper.
- h) The disc protection plates may be removed or their shape modified.

M11.2 FRONT BRAKES

The front brakes are free, provided that they are mounted on the fixation points of the original brakes and that they comply with the following prescriptions:

- a) The maximum number of pistons per wheel is four (4);
- b) The maximum diameter of the brake disc is 295 mm;
- c) The brake discs must be made from ferrous metallic material.

M11.3 REAR BRAKES

The rear brakes are free provided that they comply with the following prescriptions:

- a) The maximum number of pistons per wheel is two (2);
- b) The maximum diameter of the brake disc is 295mm.
- c) The brake discs must be made from ferrous metallic material.

M11.4 MASTER CYLINDERS/PEDAL BOX

Free. Modifications to the body shell are authorised provided they have no other function than to allow the fixing of the master cylinder and/or the pedal box. The balance of the braking forces between the front and rear axles may only be adjusted by the driver through:

- a) Direct intervention on the position of the centre of the joint, on the linkage lever of the hydraulic pumps of the front and rear circuits.
- b) Direct intervention on a proportional valve, in which the intake pressure of the rear circuit is adjusted through a pre-loaded spring, variable according to the position of the manual linkage system.

M12. BODYWORK

M12.1 EXTERIOR

Exterior decorative trim parts may be removed provided that all fixing holes are neatly filled and painted to vehicle colour.

M12.1.1 WHEEL ARCHES

The original production car contours of the wheel arch /wing must remain but may be increased up to maximum total of 30mm of the original dimensions. The organiser retains the right to reject any wheel arches that do not confirm with this article.

M12.2 INTERIOR

M12.2.1 DASHBOARD

The original dashboard moulding must be retained, although air vents and control panels inset into this moulding may be removed and neatly covered.

M12.2.2 INSTRUMENTS

The instruments are free. However, their installation should not present any risk. Standard switches may be replaced by switches of different design and may be fitted at different locations on the dashboard or on the centre console. Any openings that result from their removal must be covered.

M12.2.3 ACCESSORIES

Removal of additional accessories which have no effect on the car's behaviour, such as those which render the interior of the car more aesthetic or comfortable (lighting, heating, radio, etc.), are authorised provided that they do not influence, even in a secondary manner, the performance of the engine, steering, transmission, brakes or road-holding.

M12.2.4 INTERIOR TRIMS

All internal coverings and trims, including the headlining, sound deadening material, the standard seat belts, rear parcel shelf trim and carpets, must be removed from the cockpit, engine compartment, and luggage compartment. It is permitted to remove the weather strips from around the door and boot/tailgate openings. The inner door panels and inner side panels may be replaced by other fire resistant panels which completely cover the window winder and door catch mechanisms. These panels must be neat and cover the entire door opening.

M12.2.5 WINDOWS

- (i) Manual window winders may replace electrical system if fitted. Driver's window must remain fully functional by manual or electronic. The total mechanism may be removed from the front passenger and rear doors with the windows sealed shut. Cooling ducts for interior cooling may be added to rear side windows. It is permitted to replace the glass side and rear windows with polycarbonate, 3mm minimum thickness for side and the rear window.
- (ii) It is allowed to have net window covering the driver's door window opening forward till the centre of steering wheel if item 12.2.5 (i) above does not apply. Details:
 - (a) The window net must cover the opening forward to the centre of the steering wheel and be able to withstand any load applied at any point.
 - (b) The net may be locally modified to preserve the driver's view of the external mirror.
 - (c) The net must be affixed by means of a rapid release system so that, even with the automobile inverted it must be possible to detach the mechanism with one hand.

M12.2.6 STEERING WHEEL

The steering wheel is free, but it must be closed. The locking system of the anti-theft device must be rendered inoperative; a removable steering wheel is permitted.

M12.2.7 GEAR CHANGE MECHANISM

The gear change lever and linkage may be altered or replaced to improve the ease of use together with the pedals, which may be strengthened or replaced with stronger units. Modifications to the bodywork for the mounting and passage of the new gearshift control are authorised only if they are not at variance with other points of these regulations.

M12.2.8 OTHER PROTECTIONS

All production under-body and under-engine protection may be removed.

M12.2.9 WIPERS

The windscreen wiper system is free in location and method of operation, providing that it is fully operational at all times and capable of satisfactorily clearing the area of the screen ahead of the driver. Any rear screen wiper and mechanism may be removed.

M12.3 SPOILERS AND AESTHETIC ACCESSORIES

M12.3.1 AERODYNAMIC DEVICES

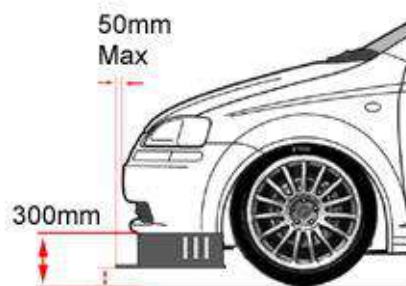
Other than those allowed in this regulation, only those aerodynamic devices which are permanently mounted on the bodywork and which are Series Production parts on the 2,500 units produced for normal road use in the country of origin are permitted. The organiser will be the sole arbiter in any dispute concerning the eligibility of aerodynamic devices.

M12.3.2 FRONT

The only body parts that can be replaced and changed in shape are:

- Front bumper; shape resembles to original and no dive planes allowed.
- Front fenders' lower edge behind the wheel may not be higher than front door's bottom border (no louvres allowed)
- Side sills (these may be added if not present in the production car)
- Non-metal material.

Front spoilers may be fitted but may not extend forward from the bodywork by more than 50mm and may not extend past the overall width of the bodywork. The areas that are defined as front spoilers are the front bumper area before the opening of the wheelhouse measuring not more than 300mm from the level ground. The underside of this spoiler may be covered and extended to the centreline of the front wheels.



M12.3.3 MINIMUM HEIGHT

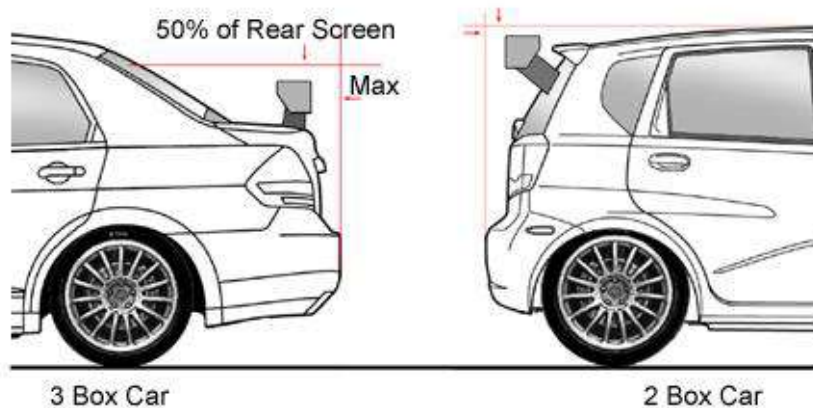
No part of the car or its suspended parts must touch the ground when two tires on the same side are deflated (5 psi pressure). No system for changing ground clearance when the car is in motion is allowed.

M12.3.4 REAR SPOILER

It is permitted to fit a rear spoiler provided that it follows the following:

- For a three box car, the spoiler must be mounted on the boot. The position of the spoiler must not be more than 50% the height of the rear screen.

- b) For a two box car, the spoiler must be mounted on the rear hatch. The position of the spoiler must not protrude above the highest point of the car when viewed from the side.
- c) The spoilers must not protrude outside the perimeter of the bodywork.
- d) The spoiler must be made in one single piece (a single profile and no adjustment flap).
- e) The spoiler must be completely contained within the front projection of the car without its rear-view mirrors.



M12.3.5 ORIGINAL EQUIPMENT BODY ACCESSORIES

It is permitted to fit original equipment accessories side sill extension and rear bumper extension.

M13. ELECTRICS

M13.1 The electrical wiring harness assembly of car must be based on the production unit. Unused wires and relays maybe removed.

M13.2 Fuses and/or circuit breakers may be added to the electrical circuit. The fuse box may be moved or removed.

M13.3 All lights must be fitted as supplied by the Manufacturer for that make and model and must be fully operative at all times. Front fog lights may be removed and the apertures must be sealed or used in accordance with Article 12.1.1.

M13.4 All rear lights including a rear fog lights must remain functional.

M13.5 Only one vehicle battery maybe fitted, make and type is free. It must be a sealed unit and may be mounted inside the vehicle. It must be possible at all times to start the engine with the energy of the battery transported on board the vehicle. Should the battery be position inside the driving compartment, it must be covered with liquid proof protective cover that fully enclosed the battery.

M13.6 Alternators are free but must remain fully operational.

M14. WHEELS & TYRES

M14.1 The maximum dimensions of the 4 rims + flanges are 8" x 15". Refer to section **B. Sporting Regulations Art. 31**

M14.2 In all other respects the wheels are free provided that they are made of cast aluminium and in a single unit. Metal inserts are allowed for the passage of the drive to the wheel.

M14.3 Wheel bolts maybe changed to a stud type however the numbers of the fixation point have a minimum number of 4.

M14.4 The tyre supplied by the tyre supplier appointed by the Organiser **MUST** be used by all competitors during the event. Refer to section **B. Sporting Regulations Art. 31**

M15. FUEL TANK & PUMP & FUEL

M15.1 Only the originally equipped fuel tank or an FIA FT3 1999, FT3.5 or FT5 or Organisers approved tank that complies with FIA (FT3 1999, FT3.5 or FT5) is allowed. An add-on fuel cell (FT3 1999, FT3.5 or FT5 or organiser's approved) on the top of the main fuel cell and installed in a housing corresponding to this regulation may be used up to the total maximal fuel volume. The total maximum capacity of the tank(s) is 60litres.

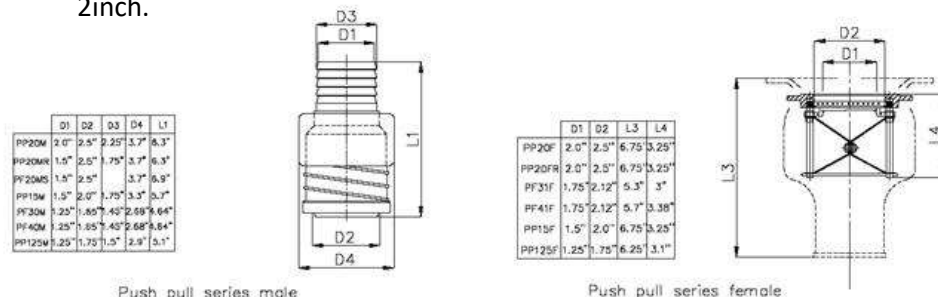
M15.2 Fuel pump(s), fuel filter(s) and fuel lines are free. Aviation quality steel braided hose or equivalent is highly recommended.

M15.3 An additional anti-surge tank maybe complemented to the fuel system. The volume of this tank must not be more than 5 litres.

M15.4 The fuel tank, pump, filters, anti surge tank and lines must be fully shielded from the driving compartment. These may be in the form of a liquid-proof bulkhead or by a liquid-proof case.

M15.5 Original fuel tank **CANNOT** be modified to suit FIA refuelling equipments. Only FIA or Organizer approved fuel tank are allowed to be use with FIA refuelling equipment.

M15.6 Only FIA approved refuelling couplings as per drawing 252-5 (Version A) are allowed to be use with the FIA refuelling rig. The internal diameter of D1 must not exceed 50mm or 2inch.



Drawing 252-5 version A

M15.7 The location of the fuel filler inlet must not be higher than the lower edge of the rear glass.

SECTION D - S-PRODUCTION (STOCK)

SPECIFIC MODIFICATION (UNDER ARTICLE 277 OF FIA APPENDIX J) VALID FOR S-PRODUCTION CAR

(The regulations are subject to change, which will be published via Additional Supplementary Regulations – ASR)

1) GENERAL SPECIFICATION

Eligible Cars are those cars of which at least 2,500 identical units must have been produced in 12 consecutive months and which have been available for sale **in Asia**. The model must be for public sale or produced in the year 2006 till 2020.

For avoidance of doubt, 'standard specification' will always be made with reference to vehicles sold through the applicable time periods. Special edition models are not eligible. Special/ Limited versions are not deemed to constitute a specification reference source for the purposes of these regulations.

Only cars with two-wheel drive (Front Wheel or Rear Wheel Drive) are eligible. Cars with forced induction are not eligible.

Complete-Built-Unit Honda FD2 (with certification) by MUGEN that complies with the Honda Exciting Cup One Make Race technical regulation without further modification (unless within these Regulations) can be accepted.

Eligible Car	S- Production 1	1901cc up to 2000cc (SMax 20)
	S- Production 2	1401cc up to 1600cc (SMax 16)

Permitted Vehicles

FIA Article 254 :	Group N Cars above 1900cc
FIA Article 255 :	Group A cars above 1900cc
FIA Article 277 :	Non FIA Homologated Touring Cars Above 1900cc (2500 minimum annual production)
FIA Article 254 :	Group N Cars 1401cc to 1600cc
FIA Article 255 :	Group A Cars 1401cc to 1600cc
FIA Article 277 :	National Series Production Cars 1401cc to 1600cc (2,500 units minimum production)

2) SPECIFIC MODIFICATIONS

S1. INTRODUCTION

S1.1 The following Technical Regulations are set out in accordance with the Organiser specified format and it should be clearly understood that if the following texts do not clearly specify that you can do it, you should work on the principle that you cannot.

S1.2 Everything that is not explicitly authorised and anything that is not specified:
(i) In these Regulations,

- (ii) In the Manufacturers' Technical manual,
 - (iii) In any official Technical Bulletin that maybe published, is strictly forbidden.
- The technical modifications specified in these Regulations relate to the use of the vehicle as a race car.

- S1.3** Competitors will be personally and solely responsible for ensuring that their cars comply with these regulations for each Event/ Round at which they are entered. Queries concerning eligibility should be referred in writing to the Organiser at least 1 Round prior to an Event/ Round entered, to permit a ruling in advance of any meeting at which it is intended to compete.
- S1.4** All Entrants/Drivers are reminded that it is their responsibility to ensure that their vehicle and equipment complies with the entirety of the Technical Regulations at all times during Official free practice, qualifying session and races and that they have taken account of all the safety requirements of these Regulations and Event in which they are participating
- S1.5** The Organiser reserves the right to issue additional statements clarifying the Regulations from time to time (Technical Bulletins). These numbered statements will be issued to all registered Entrants/Drivers by email or by formal communication at the Event (e.g. Drivers' briefing, etc.).
- S1.6** Motorsport-type bodysells (as often but not exclusively denoted via blank chassis plates, lack of underseal, lack of wax injection, lack of production bracketry etc.) are prohibited.
- S1.7** It is the intention of The Organisers to equalise the performance of the cars by setting minimum weights. The Organisers reserve the right to amend any of the minimum weights specified at any stage of the season. The minimum weights will be reviewed after each round. If changes are to be made subsequent to this review, 7 days' notice will be given. There is no appeal against the application or addition of ballast or other restrictions.
- S1.8** Examination of Vehicles
- (i) The Organisers (in addition to any other powers they may have under these regulations) reserve the right before or after any race in the Malaysia Championship Series to designate any one or more of the competing cars for special eligibility scrutineering.
 - (ii) The Competitor shall immediately place the car under the control of The Organiser/ Scrutineer and be deemed to have permitted all such scrutineering, examination, testing and any relevant procedure/ measure as The Organiser may responsibly require to undertake.
 - (iii) Competitors must be prepared, with tools and spare parts as necessary, to enable inspection of components, or to have units sealed at the circuit for later inspection by the Scrutineer at the Competitor's expense.
 - (iv) Any stripping of the engine or any required component will be undertaken by the Competitor and / or mechanic / technician nominated by the Competitor.

S2 SAFETY REQUIREMENTS

- S2.1** Kindly refer to Article 3 Safety Requirement Section A of 2022 Technical Regulation.
- S2.2** The presentation of the car is fundamental to the image of the Malaysia Championship Series. Thus cars entered must be of a standard specification appearance and exhibit all items of external trim associated with the appropriate eligible model.
- S2.3** In case of serious accident, an alternative age or model body shell may be used provided:
- (i) no weight or other benefit is derived,
 - (ii) the finished car's appearance corresponds with that of the eligible model. The age of the replacement bodyshell dictates the type of external trim to be fitted in order that the car does not present the image of a hybrid, and
 - (iii) the use of titanium, ceramics, carbon fibre, Kevlar, composites thereof (with the exception of the Drivers' seat) and magnesium is prohibited.
- S2.4** Only parts listed at the time of manufacture of the vehicle may be used. If parts have been superseded since the date of vehicle manufacture the replacement part may be used prior consent from The Organiser. Competitors must provide the detail of parts should he be required to.

3. CHASSIS

- S3.1** No chassis stiffening is permitted except that derived from the fitting of a roll cage. Minimum six mounting feet inside the car are permitted, and a 3mm thick plate welded to the chassis to which the cage should be bolted/ welded. Seam welding, strengthening of mounting points and changes in component material of the bodyshell or panels is prohibited. Bodywork repairs to be conducted in structural areas should involve Competitor dialogue with the Malaysia Championship Series Scrutineer to ensure ongoing bodyshell compliance.
- S3.2** The following modifications are prohibited:
- (i) Unless specifically authorised in these regulations, the lightening of components or panels in any way, whether via removal of metal or otherwise is prohibited. Specifically but not exclusively this includes removal of any unused bracketry welded to the bodyshell or other components: seat rails, seat front cross member, rear seat base frame, roof tin supports, seat belt anchor backplates, rear seat hinge and retaining brackets. Removal of any unused bracketry rear of the main roll over bar is permitted.
 - (ii) Inner wheel arch modification is prohibited and material may not be removed to allow clearance for suspension components, driveshafts and linkages.
 - (iii) Front turret central cone repositioning.
 - (iv) Removal of bonded metal plugs in the floorplan or other areas of the interior and exterior.

S4. BODYWORK and MEASUREMENTS

- S4.1 Modifications:**
- (i) In general it must be of the standard specification for the vehicle, with no additions or omissions. Mandatory fitment of laminated windscreen. Seam

- welding, strengthening of mounting points and changes in component material are prohibited.
- (ii) Interior: Must be of the standard specification for the vehicle. Rear quarter panel trims, boot side panels, tailgate trim panel and weather proofing plastic sheets can be removed. A basic door trim manufactured from plastic or aluminium only must be fitted to the inside of the Driver's door – this must be to a high standard with no sharp edges and a smooth surface. Windows must be fully operable – either manual lever or electric types allowed.
- Driver's seat must be replaced with the competition seat fitted.
- Passenger and rear seats must be removed. Headlining and carpets must be removed. The inner sunroof steel sleeve and mechanism may be removed. Dashboard facia must remain standard with the exception of minimal material trimming to accommodate rollcage fitment around the A-pillars, but must otherwise remain as originally fitted. **Air-Conditioning System can be removed (Air Conditioning Vent Panel must be remained).** An interior rear view mirror must be fitted.
- (iii) Exterior: As per standard specification for the eligible vehicle as manufactured.
- The bonnet must be secured by surface-mounted bonnet pins; the rear hatch must be secured either by surface-mounted bonnet pins and locks can be removed.
 - Unless specifically authorised in these regulations, the use, substitution of, and / or addition of, any parts, material or processes is prohibited, other than welding or repairing material or the fitting of standard replacement parts, for the sole purpose in every respect of restoring the vehicle to the manufacturer's standard specification or to comply with safety requirements.
 - All, door rubbing strips, weather strips/channels must be retained.
 - Two external rear-view mirrors, one on the left-hand side and one on the right hand side must be fitted and be a standard part pertaining to that model.
 - Bumper mountings are free provided the bodywork and the shape and position of the bumpers remain unchanged.
 - Bonnet / engine cover must remain in the normal position and, when closed, must not have any non- standard gaps at any of their edges. Fitment of tape etc. to close body gaps (e.g. bonnet to wing) is prohibited.
 - It is not permitted to increase the width of the wheel arch by the addition of material to, or the deformation of, the outside or inside of the wheel arch. No additional holes may be cut into the bodywork or spoiler to aid cooling, brake ducting, air intake etc. Apertures may not be enlarged.
 - The removal of underseal from vehicle wheel arches and any areas outside the vehicle wheelbase is prohibited.**
- (iv) It is permitted to alter the ride height. Under no circumstance can any part of the bodywork, or of the suspended part of the car including the exhaust system, be below a horizontal plane passing 70mm above the ground, the car being in normal racing trim with the Driver in the normal seated position.

S5. ENGINE

Must be of the standard type and specification for the exact vehicle and made of the standard material. All components must be used in their production positions, with no additions or omissions unless expressly stated in these regulations. No modifications are permitted. It is the Competitor's responsibility to ensure that no prohibited modifications have been carried out, even if they are using an engine not assembled by them.

S5.1 ENGINE MODIFICATIONS

- (i) Reciprocating parts may not be altered in anyway, this includes lightening or balancing.
- (ii) Replacement valves & valve guides may be fitted but must be production or standard (shape, profile and material).
- (iii) Fitment of oil catch tank, **of minimum capacity 1 litre.**
- (iv) The fitting of higher specification grade con-rod cap bolts.
- (v) Cylinder head - MUST remain to standard specifications. No material may be added to or removed from the cylinder head; inlet and exhaust ports may not be modified in any way and must remain as cast and as per standard specification.
- (vi) The head casting material cannot be ground, smoothed, welded or in any way altered, whether to achieve optimisation of gas flow or otherwise. Any attempt to alter the shape of the ports, valve throats or the valve seats is prohibited.
- (vii) Standard pistons may not be replaced by forged pistons or 'machined-from-billet' pistons. Piston crowns must not be machined from standard in shape or profile.
- (viii) Valve sizes must not be altered from standard specification.
- (ix) Uprated lifters and lightweight spring caps are prohibited.
- (x) Fitment of an oil pump from a different production model is prohibited.
- (xi) Modification of oil pick-up pipe prohibited.
- (xii) Fitment of a nonstandard head gasket whether sourced from other vehicles within the model range or elsewhere is prohibited.
- (xiii) Fuel injectors cannot be substituted for non-standard parts.
- (xiv) The swapping of engine components or any ancillary components between models is prohibited. This is inclusive of any components in the engine bay such as, but not exclusively, pistons and electronics.
- (xv) Standard inlet manifold must be retained and may not be modified by the removal or addition of material.

S5.2 LOCATION

Position and mounting method must be of standard specification. All engine mounts must be of standard design and material rigidity grade. Voids in these standard engine mounts cannot be filled with any material to attempt to stiffen the mounts. No additional engine/transmission supports may be fitted, whether chassis mounted, or otherwise.

S.6 COOLING SYSTEMS

Water radiators must remain of standard mass production for the model being raced, in design, size and position. The standard radiator fan and fan cowling must also be retained. No modifications are permitted. Radiator conversion within family brand is allowed (i.e MT Radiator to AT Radiator)

S7. INDUCTION SYSTEMS

The complete induction system must be of the standard specification for the vehicle and be retained in its entirety; Components must be used in their standard positions, with no additions or omissions except for the following:

- (i) Aftermarket air filter can be used to replace the standard part. This must be of the correct specification for the model to which it is fitted.
- (ii) No other modifications are permitted.

S8. EXHAUST SYSTEMS

- (i) Exhaust emission control devices such as air pumps, associated lines, nozzles, canisters, and electrical/mechanical devices may be removed. Any holes remaining after removing such devices shall be completely plugged. If fitted catalytic converter(s) may be removed.
- (ii) Exhaust manifold(s) are unrestricted. Exhaust tubing design is unrestricted, as long as the exhaust exits behind the driver, directed away from the car.

S9. ENGINE MANAGEMENT – Fuelling, Ignition & Valve Timing

Components must be of standard specification for the vehicle and used in their standard positions, with no additions or omissions. Remapping of ECU is allowed. **Piggy back is allowed only to Toyota Vios 1.5.**

S10. SUSPENSION

- 1) SP1
Shock Absorber and its Spring is free.
- 2) SP2

The standard specification suspension configuration must be retained and the standard mounting points and position of the suspension components to the bodyshell and related suspension units must be used and remain as standard. All suspension components (uprights, wishbones, hubs, bottom ball joints, suspension top mounts, steering arms, rear beams, anti-roll bars, supports bolted to the bodyshell or subframe) must be standard, unmodified and remain in their standard position, with no omissions, additions, lightening or modifications allowed to the mounting points and be manufactured from the standard material type except where specifically allowed in these regulations. No additions or modifications may be made to the mounting points. Suspension must be controlled to avoid fouling of wheels on chassis or bodywork.

- (i) Modifications to the suspension pick up points are prohibited.
- (ii) Modification of the engine subframe or its mounting position prohibited.
- (iii) Spherical joints, rose joints and rod ends are prohibited.
- (iv) Seam welding of front wishbones or engine subframe is prohibited.
- (v) Dampers other than the control items, or control items with identification tags missing are prohibited.

- (vi) Any mechanism for changing the suspension geometry, other than ride height and camber, is prohibited
- (vii) No additional springing medium may be utilised.
- (viii) Rear suspension and stub axles must remain standard; no strengthening or addition / removal of material is permitted.

S11. WHEELBASE /TRACK

Wheelbase and track must remain as standard for the vehicle and separately, the acceptance of any minute changes arising solely from adjusting ride height and camber, within the scope of these regulations.

S12. TRANSMISSIONS

Must be of the standard coded type and specification for the vehicle. The method of clutch actuation must not be altered in anyway.

For clarity, final drive and gear ratios must remain as per standard and it is prohibited to mix and match any components. Gearbox casing codes must correspond with the gear ratios contained within. Entry/ Entrance need to submit their gearbox ratio and final drive as a declaration.

Gearbox casings cannot be altered by the addition or removal of material, internally, or externally. No modifications to driveshafts or constant velocity joints are permitted. The gearbox must always include a reverse gear, which can be engaged by the Driver sitting in his/her seat with his/her safety harness fastened.

Aftermarket Limited Slip Differentials are allowed.

S13. ELECTRICS

S13.1 To standard manufacturer's specification for the vehicle which must all be fully functional. Spotlights and bumper mounted side lights may be removed.

S13.2 Electrical and Electronic Systems Modifications

- (i) The rear wiper, motor, mechanism and wiring may be removed. Rear washer, plumbing, bottle, wiring and switch may be removed.
- (ii) Removal of interior light electric components.
- (iii) Addition of auxiliary gauges and switches. The main speedometer binnacle must be standard. Any openings which result from alterations must be covered.
- (iv) The standard wiring loom must be retained
- (v) The vehicle air bags must be disabled and may be removed.
- (vi) All other electrical and electronic systems must remain in their standard positions and be of standard specifications.
- (vii) Substitution of ignition coils with high power ignition coils is prohibited.
- (viii) Fitment of non-standard distributors is prohibited.

S14. BRAKES

S14.1 The vehicle's original brakes may be only be modified as follows:

- 1) Smax 20 (SP1)
 - i. Brake calipers, rotors, hubs, hoses are unrestricted and mounting can be adjusted. Brake rotors may only be of a ferrous material.
 - ii. Any brake cooling method may be used. No modifications to the original bodywork are permitted, except that bumper grilles may be removed and existing openings may be utilized.
 - iii. Removal of handbrake system is allowed.
- 2) SMax 16 (SP2)
 - i. Brake calipers, rotors, hubs and hoses are unrestricted, provided that the brake components are attached at the manufacturer's original mounting points and that no modifications to the original mounting points are made. Brake rotors may only be of a ferrous material.
 - ii. Any brake cooling method may be used. No modifications to the original bodywork are permitted, except that bumper grilles may be removed and existing openings may be utilized.
 - iii. Removal of handbrake system is prohibited.

S15. WHEELS & TYRES

SMax 20 - Kindly refer to Art. T7 Section B T-Production
 SMax 16 - Kindly refer to Art. M14 Section C M-Production

S16. VEHICLE WEIGHT

The principle is to equalise the performance of the cars. The minimum weights must be respected at all times during an Event. This is measured with a maximum of 3 litres of fuel. Ballast must be added inside the car with mounting points using all four fixing bolts. It is the Competitor's responsibility to provide and fit their own ballast if it is required. Minimum Weight (excluding Driver) =

S16.1 SMax 20 1150 kilograms

S16.2 SMax 16 General Model

1500 cc	1020 kilograms
1600 cc	1050 kilograms

Specific Model	
Toyota Vios 1.5 (2NR-FE)	1000 kilograms
Honda GK5 1.5 – CVT (L15Z2)	1010 kilograms
Honda GK5 1.5 – MT (L15B)	1050 kilograms
Swift ZC32s (1.6cc) - MT	1030 kilograms
Swift ZC31 (1.6cc) -MT	1010 kilograms
Swift ZC32s (1.6cc) - AT	1020 kilograms

(Weight may be adjusted if it necessary to balance the lap times if needed for competitiveness purposes.)

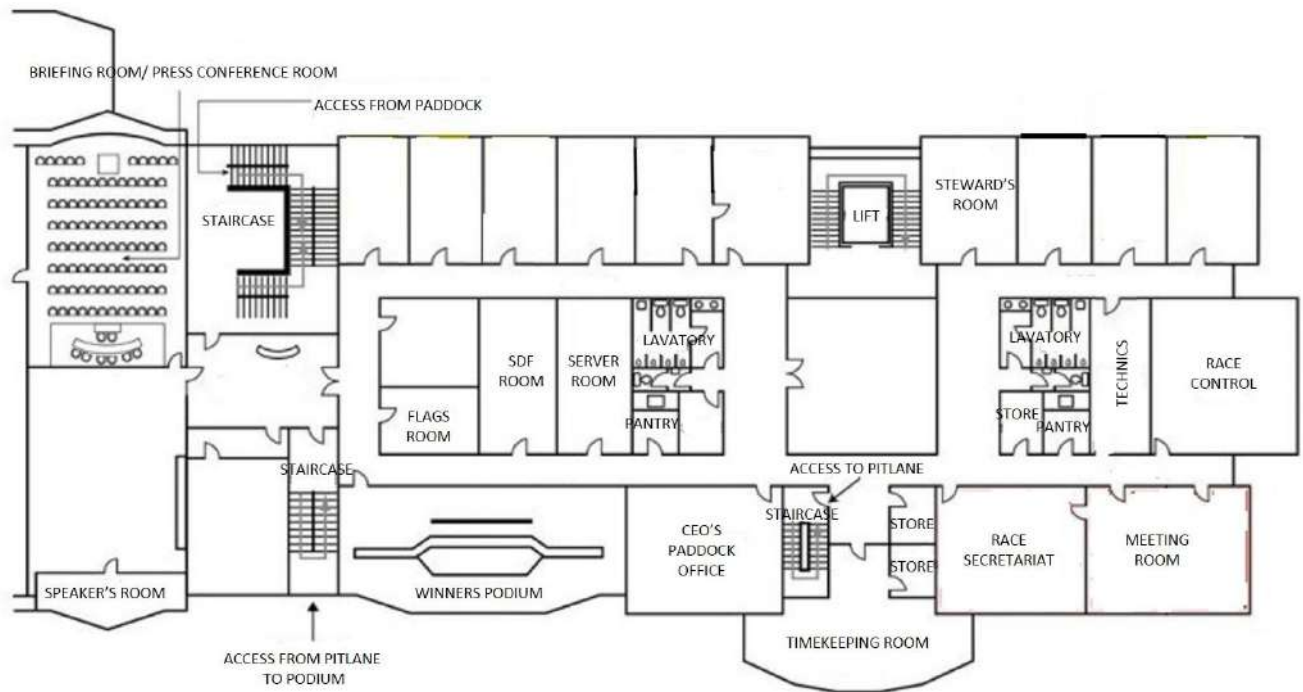
S17. FUEL TANK / FUEL

- S17.1 Standard fuel tank must be retained and utilised. Additional baffles or foam in the tank are not permitted.
- S17.2 Tank location and fuel pump must be standard
- S17.3 Fuel: RON 97 sold at the Circuit.



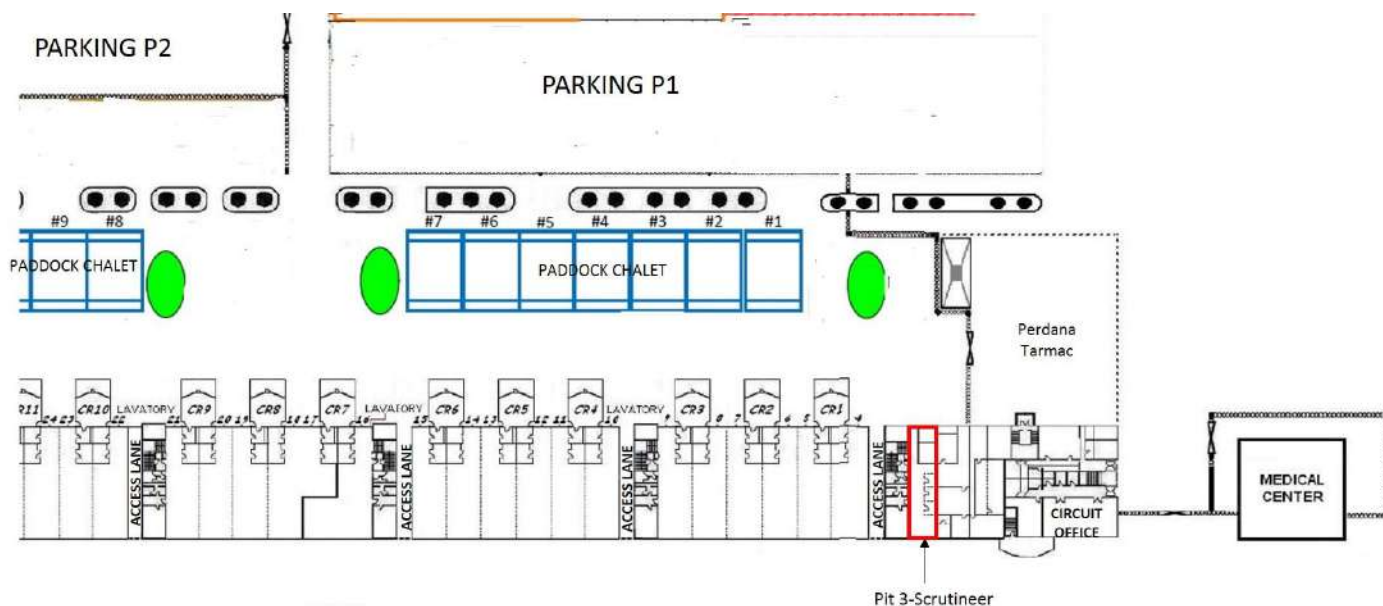
APPENDIX I

PRECISE LOCATION OF OFFICES - First Floor

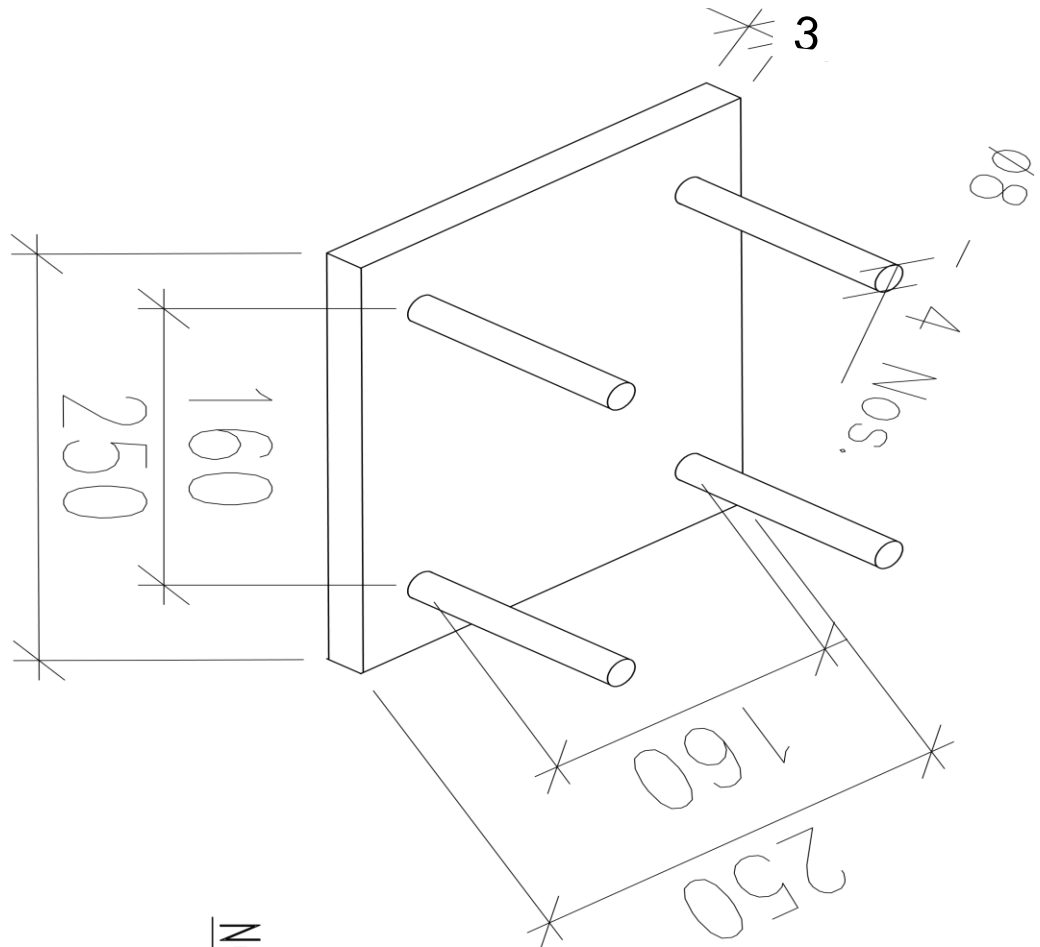


APPENDIX II

SCRUTINEERING BAY (Pit 3) PARC FERME (Perdana Tarmac)



APPENDIX III



NOTE :

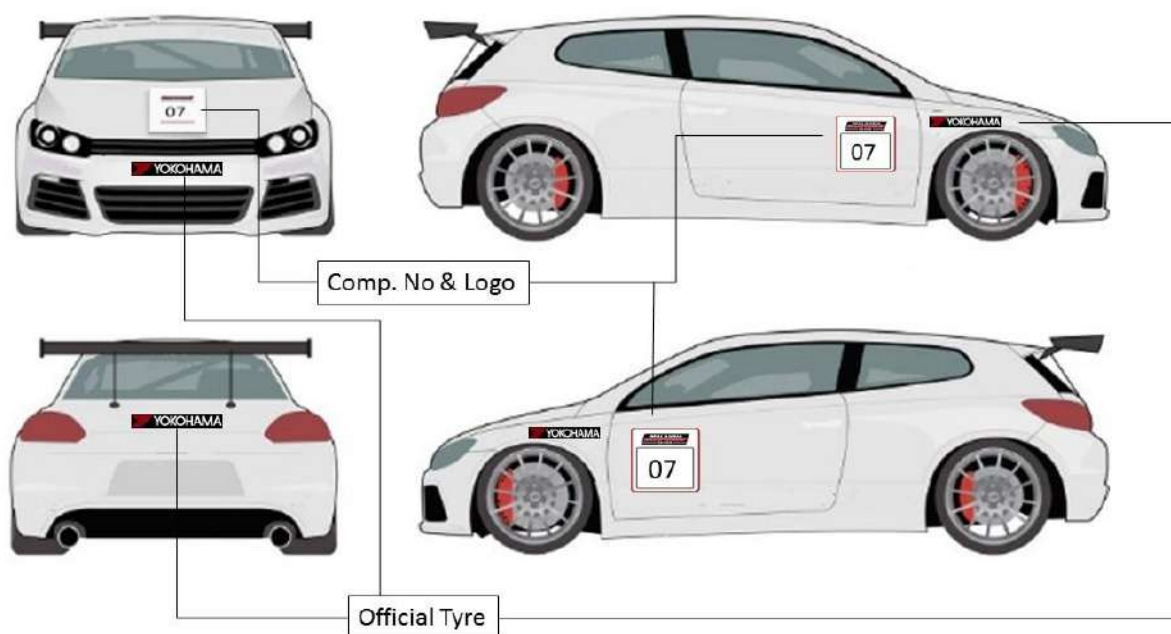
- 1- Physical Dimension
250mm(W) X 250mm(L)
With Tolerances +/- 5mm
- 2- Physical Thickness
16mm(Thk) With Tolerances
+/- 2mm



APPENDIX IV

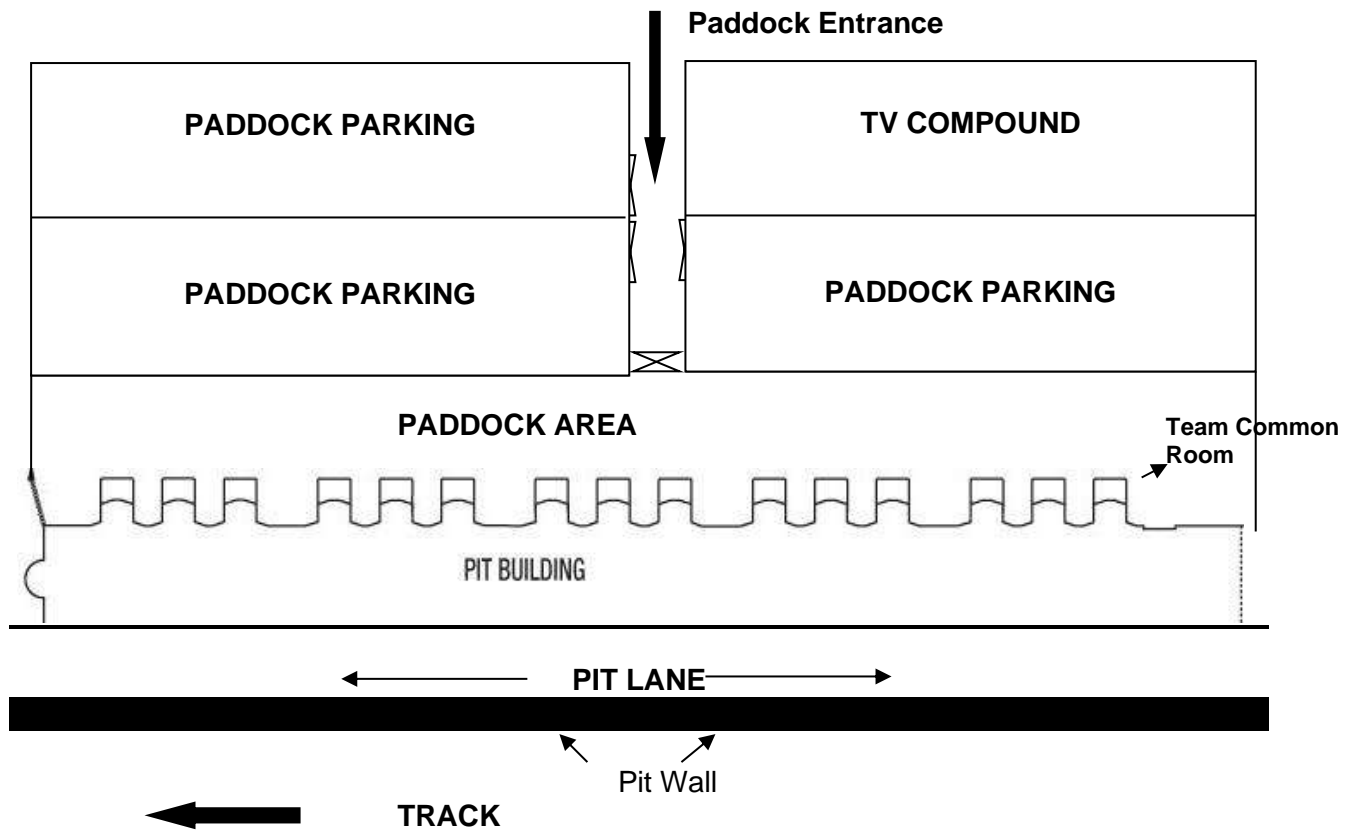
Art. 32 ADVERTISING

DECALS PLACEMENT



APPENDIX V

Art. 26 PITWALL / PITLANE / PITS / Paddock Areas AND PARKING AREAS



APPENDIX VI

2022 MALAYSIA CHAMPIONSHIP SERIES

HELD UNDER THE INTERNATIONAL SPORTING CODE OF THE FIA AND BULLETINS, GENERAL PRESCRIPTIONS, THE SERIES REGULATIONS AND ANY ADDITIONAL SUPPLEMENTARY REGULATIONS WHICH MAY BE ISSUED

COMP NO.

RACE ENTRY FORM

ROUND:

DATE:

CIRCUIT:

RACE CATEGORY:

ENTRANT			
Entrant's Name:		Entrant's License No:	
Address:			
Tel. No:		E-mail	
DRIVER 1			
Driver's Name:		NRIC/ Passport No:	
Address:			
Competition License No:		Nationality:	
Tel. No: H/P No:		E-mail:	
DRIVER 2			
Driver's Name:		NRIC/ Passport No:	
Address:			
Competition License No:		Nationality:	
Tel. No: H/P No:		E-mail:	
VEHICLE			
Make/ Model		Year	CC:
Bore and Stroke:			No. of Cylinder(s):
Competition No:			Class:

This ENTRY FORM must be completely filled in. The INDEMNITY FORM must be duly completed and signed by the Entrant and Competitor(s).

INDEMNITY DECLARATION

1. I/We have read the Supplementary Regulations issued for this meeting and agree to be bound by them and by the National Competition Rules of the Motorsports Association of Malaysia. In consideration of the acceptance of this entry or of our being permitted to take part in this meeting. I/We agree to save harmless and keep indemnified the Organiser(s), the Promoter(s), Motorsports Association of Malaysia, Circuit Owner(s), Sponsors(s), Donors(s) and their respective officials, servants, representatives and agents, from and against all actions, claims, costs, expenses for any eventualities, death and/or personal injury to myself/ourselves or loss or damage to the property of myself/ourselves and driver(s), passenger(s) or mechanic(s) and ALL members of my/our team (as the case may be) however caused arising out of or in connection with this entry or my/our taking part in this meeting and not withstanding that the same may have been contributed or occasioned by the negligence of the said bodies, their officials, servants, representatives or agents.
2. I/We declare to the best of my/our belief that the rider(s)/driver(s) possess(s) the standard of competence necessary for an event of this type to which this entry relates and that the vehicle entered suitable and roadworthy for the event having regard to the course and the speeds which will be reached.
3. I/We declare that I/we and all other persons in any way connected with this entry recognize and accept that the jurisdiction in all matters arising out of this race meeting is vested in the organizers and the Motorsports Association of Malaysia under the International Sporting Codes of the FIA.

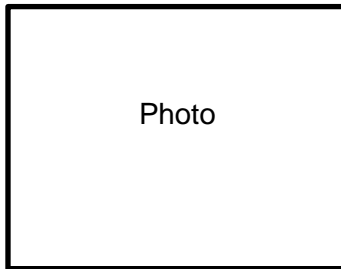
SIGNATURE		
Name of Entrant:	Signature of Entrant:	Date:
Name of Driver 1 :	Signature of Driver:	Date:
Name of Driver 2 :	Signature of Driver:	Date:

The above declaration must be signed by parent(s)/legal guardian if the competitor is under the age of 18 as at the date of this entry.

Name of Parent/Legal Guardian:	Signature of Parent/Legal Guardian:
NRIC/Passport No:	Relationship with Minor:
Tel. No:	Fax No:

Foreign competitors are reminded that permission must be sought from their respective ASNs to participate in this meeting either in writing or by having this Entry Form stamped in the allocated box.

**ASN'S
STAMP**



APPENDIX VII

Each Competitor should complete this form in full, giving an answer in each box, and using block capitals wherever possible and submit to Race Secretariat for media purpose.

Driver's Name :	Category :
Nickname (if any) :	Nationality :
Helmet Colour :	Occupation :
Primary Sponsor :	
Additional Sponsor(s) :	
Website : or Email	

RACING CAREER DETAILS (Kindly attached resume if available)

Year	Championship / Series	Overall Ranking